



# Chapter 3: Greenway Network

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## OVERVIEW

For the purposes of this Plan, the greenway network refers to natural areas containing walkways and bikeways that are separated from the roadway. It does include sidepaths which may, at times, be within the roadway right-of-way. The greenway network types are woodland/natural surface hiking trails, unpaved improved trails, shared-use, paved greenways, and mountain bike trails largely determined by existing land use and anticipated human use. The four key greenway spines identified in this Plan are the Mountains-to-Sea Trail (MST), Stoney Creek Greenway, Little River Trail, and the "Ditch" greenway. Greenways are an essential part of a comprehensive walking and biking network due to their attractiveness and desirability to a wide range of users, safety, and ease of use. This chapter describes the greenway types, includes the greenway network maps, and features individual greenway sections with additional mapping and cost estimates.



*Older Mountains-to-Sea Trail (MST) signage along Stoney Creek. The MST was designated along the Stoney Creek in the 1991.*

## METHODOLOGY FOR GREENWAY NETWORK DESIGN

The recommended greenway network was designed for all types of trail users with a special focus on providing a connected network of hiking trails, unpaved greenway trails, and shared-use paved greenways. The network was developed based on Steering Committee input, public input, NCDOT Division input, recommendations from previous studies, noted destinations, presence of existing local and regional greenway projects, and field analyses.

### The Hub + Spokes Model

The image at left shows some of the key components for the overall bicycle, pedestrian and trail network based on a model of hubs (destinations) and spokes (walking and bicycling corridors).

The image below *conceptually* shows how this model of hubs and spokes could be applied in Goldsboro, NC, with a network of complete streets (in grey) and greenways (in green) connecting key destinations throughout the city. **Keep in mind the map below only conceptually shows these linkages. See maps on the following pages for actual greenway network recommendations.**





## GREENWAY TYPES

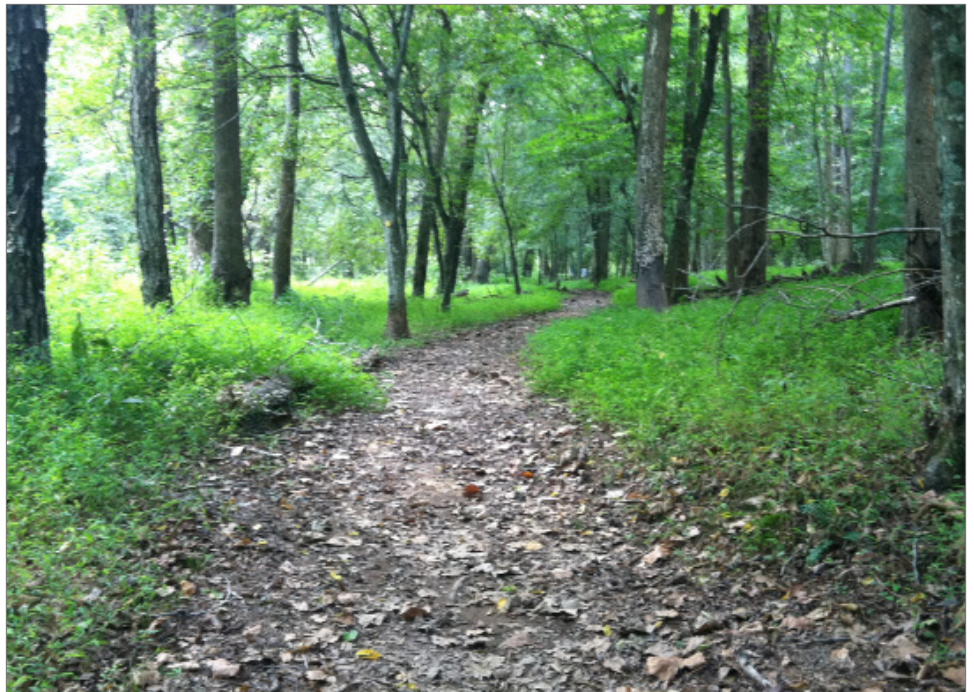
### Type I: Hiking Trails

For this study, hiking trails are defined as natural surface trails generally with soft surface and minimal improvements. The width is typically 1.5-4 feet. Examples in this study include the rural sections of the Mountains-to-Sea Trail.

*Hiking trail in  
Greensboro, NC.*



*Mountains-to-Sea Trail  
at Eno River State Park  
in Durham, NC*



## Type II: Unpaved, Improved Greenway Trails

For this study, Improved Greenway Trails refer to improved, unpaved trails generally with gravel composite. The width is typically 8-12 feet. Examples in this study include sections of the Stoney Creek Greenway and the Mountains-to-Sea Trail.



*The Wake County, NC section of the American Tobacco Trail*



*The Virginia Creeper Trail near Damascus, VA*



### Type III: Shared-use Paved Greenways

For this study, Shared-use Paved Greenways refer to paved pathways meant for a mix of bicycle and pedestrian traffic. These greenways can be used effectively for both recreation and transportation. The width is typically 10-14 feet. Examples in this study include sections of the Stoney Creek Greenway, the “Ditch” greenway, and other urban/suburban sections.

*The Tar River  
Greenway in  
Greenville, NC*



*The City of  
Durham  
section of the  
American  
Tobacco Trail*



### Type IV: Mountain Bike Trails

Mountain bike trails are trails specially constructed for mountain biking. Trails may be single track or wider. The existing Goldsboro mountain bike trails can be found in Stoney Creek Park North.



Blue Clay Mountain Bike Park in Wilmington, NC (Photo courtesy of <http://sirbikesalot.com/>).

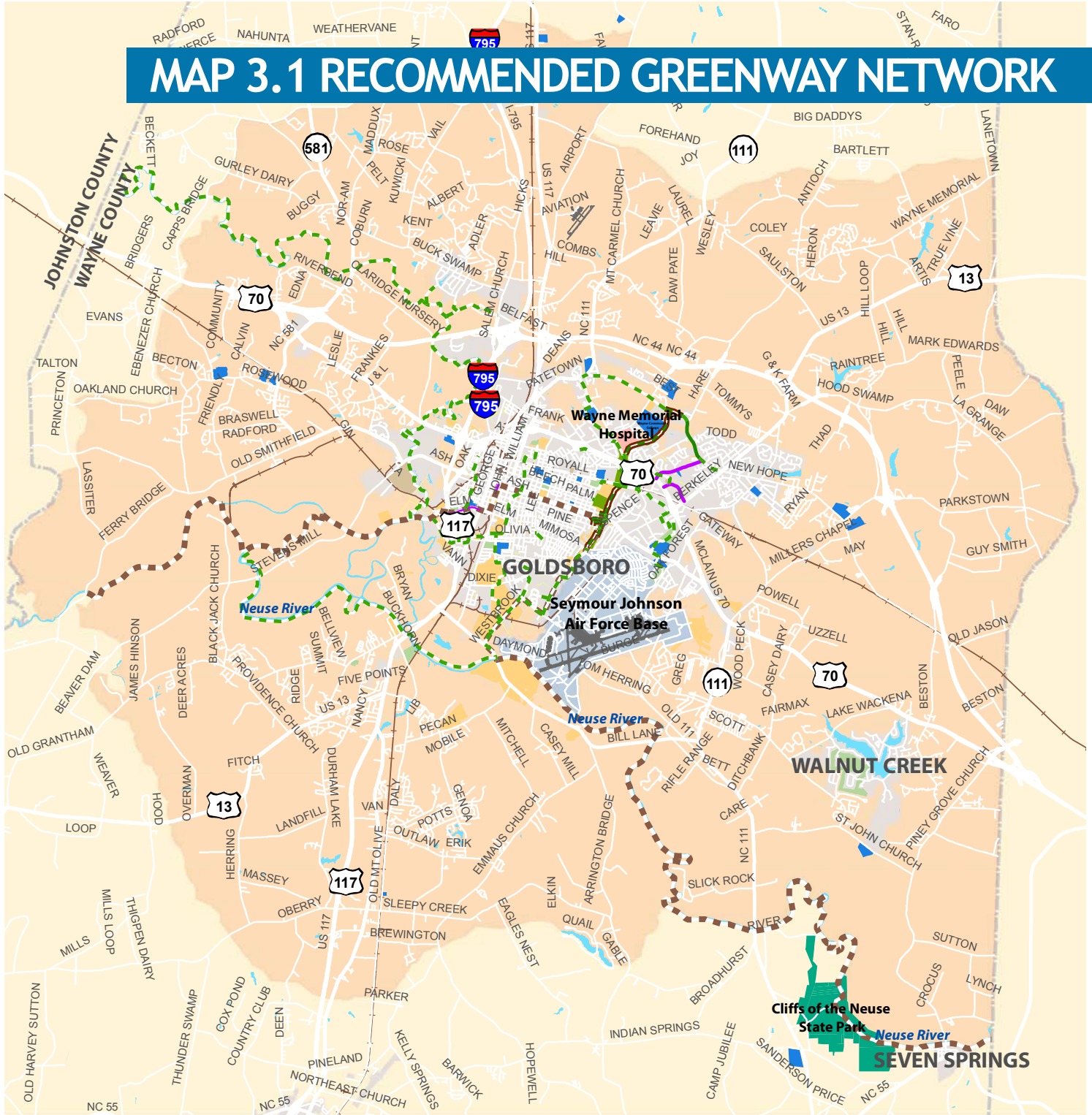
## RECOMMENDED GREENWAY NETWORK

The recommended greenway network connects existing greenways, schools, parks, neighborhoods, and other destinations while also linking to existing and recommended bikeways and walkways (described in Chapters 4 and 5). Some greenways are short but make key connections between communities. The Stoney Creek Greenway traverses Goldsboro as a regional destination while the Mountains-to-Sea Trail is a statewide trail that connects the Appalachians to the Outer Banks. The total mileage of recommended greenways is featured in the table below. The major spine and priority greenways are described by cutsheet and segment map starting on page 3-10. Maps of the recommended greenway network are portrayed in Maps 3.1 and 3.2.

<i>Greenway Type</i>	<i>Mileage</i>
Type I: Hiking Trails	41.3
Type II: Improved, Unpaved Trails	14.3
Type III: Paved Shared-use Trails	26.2
Type IV: Mountain Bike Trails	(Maintain/expand)



# MAP 3.1 RECOMMENDED GREENWAY NETWORK



## Recommended Greenway Facilities - MPO



### Legend

#### Recommended Greenways

- Shared-Use Path

#### Existing Infrastructure

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

#### Lands of Interest

- City Greenway Lands
- Park
- State Park
- Schools
- Hospital
- Other Destinations
- City Easements

#### Additional Context

- Water Bodies
- Proposed Mountains-to-Sea Trail Alignment
- Seymour Johnson Air Force Base
- City Limits
- MPO Limits
- Wayne County Border





# MAP 3.2 RECOMMENDED GREENWAY NETWORK (DOWNTOWN)



### Recommended Greenway Facilities - Downtown



#### Legend

##### Recommended Greenways

- HAWK Signal
- Shared-Use Path

##### Existing Infrastructure

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

##### Lands of Interest

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- City Easements
- Other City/County Property

##### Additional Context

- Proposed Mountains-to-Sea Trail Alignment
- Water Features
- Seymour Johnson AFB
- City Limits
- MPO Limits

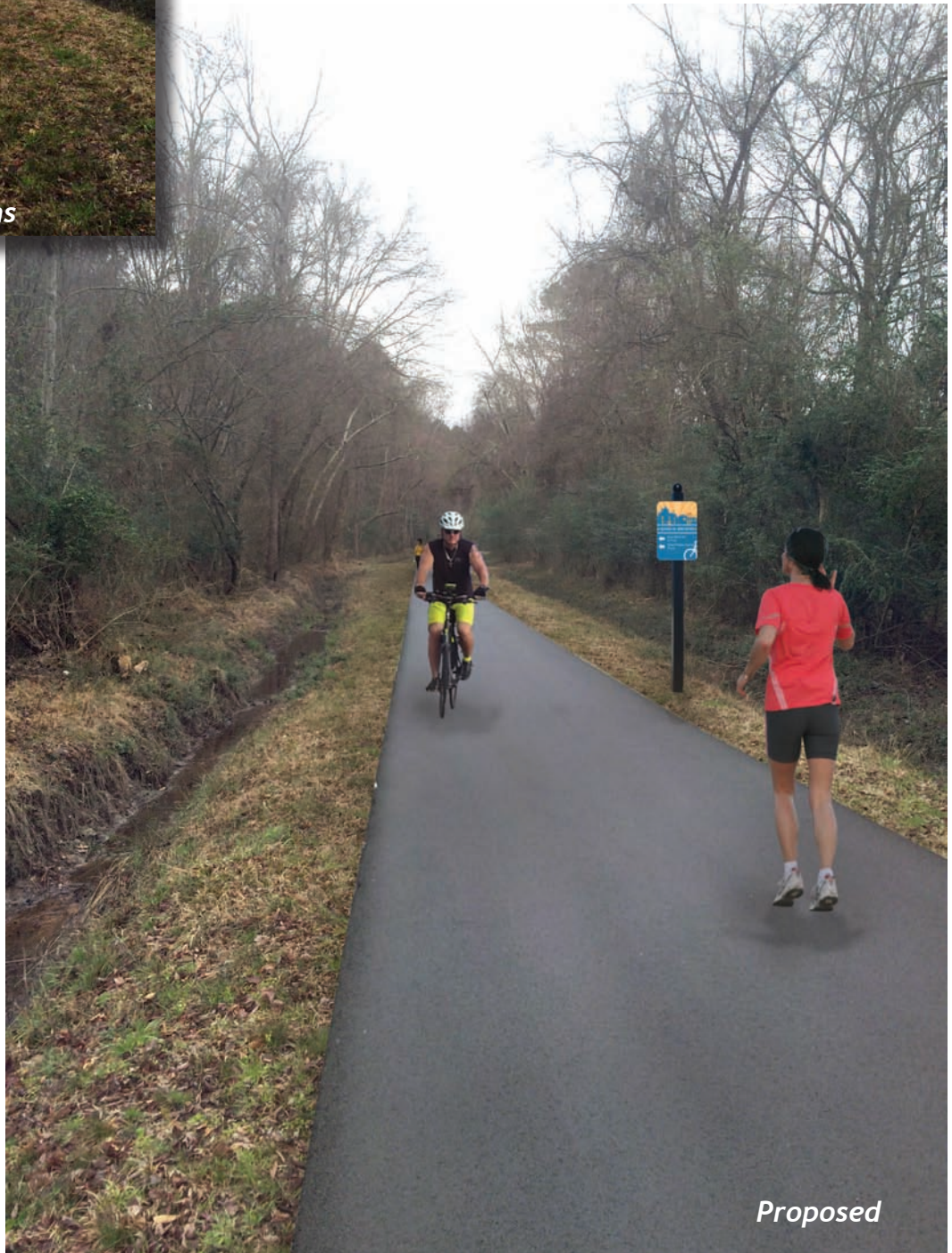






*Existing Conditions*

*A photo visualization of a section of the Stoney Creek Greenway south of Elm Street. See Stoney Creek Segment 3 on page 3-14 for more information on this section.*



*Proposed*

## STONEY CREEK GREENWAY SECTION 1

**Trail Distance:** 1.31 miles/6,916 feet

**Trail Type/Surface:** Type III. This section of trail will be predominantly asphalt with the potential of boardwalk and/or concrete as the trail nears the hospital. The material surface will be dictated by the floodway/floodplain and subsurface conditions.

### Overview

Section One is the northernmost section of the proposed Stoney Creek trail and is located along Reedy Branch; it begins at New Hope Road continuing south to Highway 70 along Reedy Branch where it intersects Stoney Creek south of the Hospital. This is a critical connection as it links several neighborhoods, churches, the Wayne County Community College, and Wayne Memorial Hospital to each other and the trail. A direct connection should be formalized from this section of greenway to the Wayne Community College trails and Wayne Memorial Hospital.

### Planning-Level Cost Estimate

The cost for trail construction only (based on an average cost of \$500K/mile) comes to \$655,000. Other major costs related to this project could include substantial sections of boardwalk in wetter areas. This could raise cost dramatically.

### Recommended Next Steps

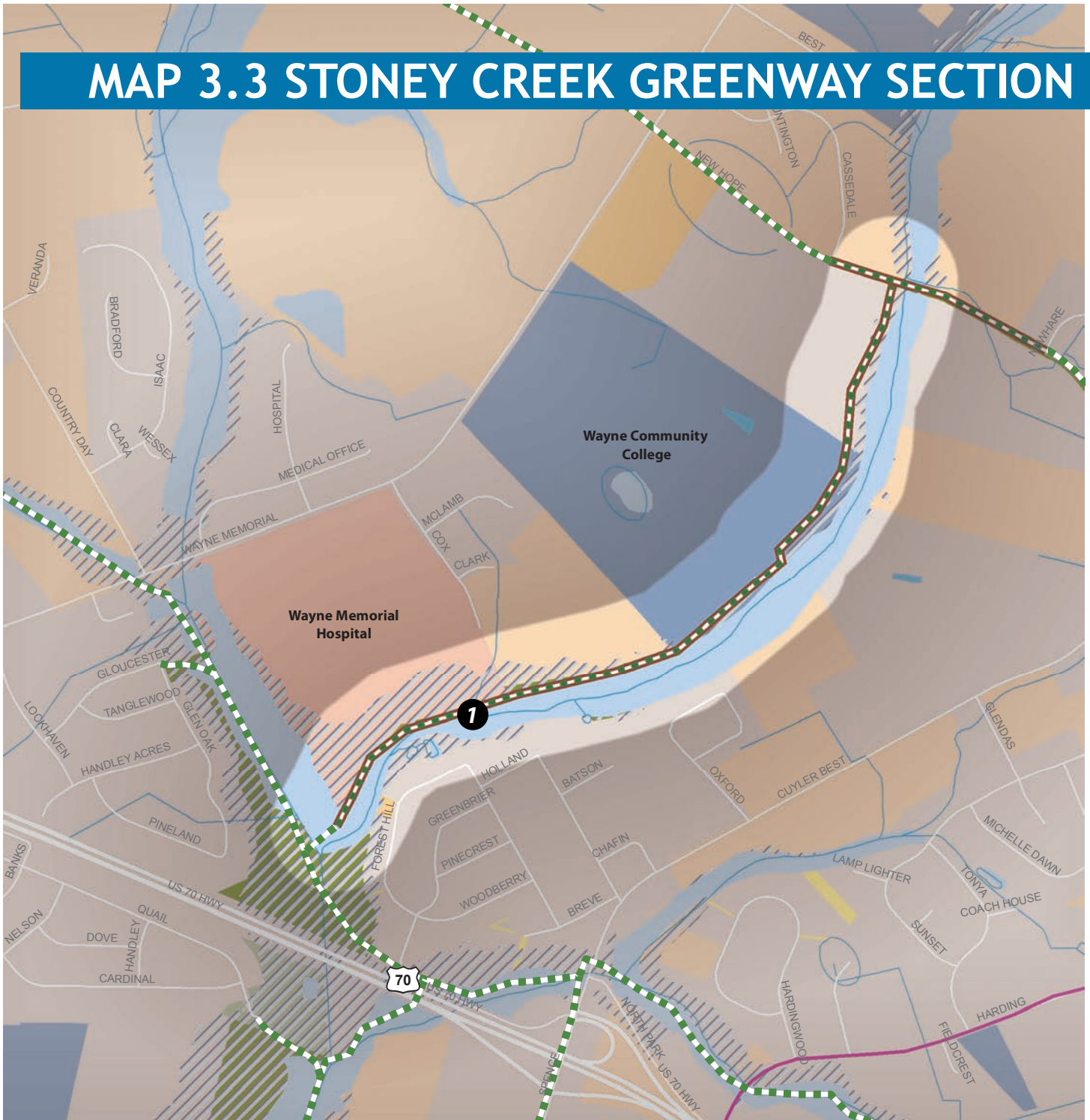
- Conduct delineation of floodway and floodplain to determine permit requirements
- Conduct geotechnical investigation to determine material needs and trail cross-section
- Secure partnerships from adjacent landowners for funding and grant application (the Community College and the Hospital).



*Cleared easement behind Hospital on wet, winter day. After further analysis and delineation, boardwalk may be the best, sustainable surface for portions of this trail.*



# MAP 3.3 STONEY CREEK GREENWAY SECTION 1



Stoney Creek Greenway - Section 1



**Legend**

**Recommended Greenways**

- Shared-Use Path

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- City Easements
- Other City/County Property

**Additional Context**

- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits



## STONEY CREEK GREENWAY SECTION 2

**Trail Distance:** 1.29 miles/6,811 feet

**Trail Type/Surface:** Type III. Asphalt with a Pedestrian Bridge Crossing over Stoney Creek (Phase 1); Phase 2 would include a Pedestrian Bridge of Highway 70

### Overview

Section Two begins at Highway 70 and continues south to Ash Street. This section has several challenges, including the crossing of Highway 70, Royall Street, and an existing railroad bridge. Once south of the railroad, there is a clear connection established to Ash Street that will link to existing neighborhoods, parks, and the mountain biking trails accessed from East Peachtree Street. It is proposed that the crossing of Highway 70 be developed in phases, first using either the existing Spence Street Bridge (and creating a sidepath along Spence to Royall) or connecting to Wayne Memorial Drive, using the Wayne Memorial/US 70 Bridge, and connecting back along Quail Drive. Not only will this provide a temporary connection, but it will improve access to existing commercial destinations where pedestrian, bike, and vehicular conflict currently exists. A controlled intersection at Spence and Royall would also be needed. The proposed trail is located within existing utility easements south of Royall and the railroad bridge. A pedestrian bridge is needed to cross Stoney Creek. Phase 2 of this connection would be planning for a bridge crossing over Highway 70, under the existing railroad, and completing a study to provide a controlled crossing at Stoney Creek and Royall Street.

### Planning-Level Cost Estimate

The cost for trail construction only (based on an average cost of \$500K/mile) comes to \$645,000 (alignment along creek not including bridge). An overpass over US70 would likely cost \$2-4 million. Other major costs related to this project would include alternative routing as described above.

### Recommended Next Steps

- Coordinate with NCDOT on future Pedestrian Crossing over Highway 70.
- Begin discussions and coordination efforts with the railroad on future rail line improvements to accommodate a trail under the bridge.
- Complete a study on the best alternative routing to US 70 Bike/Ped-Only Overpass.



*Left: One of the major obstacles for the Stoney Creek Greenway are the Royall Avenue and Railroad bridge crossings of the creek. The crossing will require further study or should be improved with any upgrades to the railroad bridge or roadway.*

*Right: The sewer easement north of Royall is ideal for the greenway.*





# MAP 3.4 STONEY CREEK GREENWAY SECTION 2



**Stoney Creek Greenway - Section 2**



**Legend**

**Recommended Greenways**

- Shared-Use Path
- HAWK Signal

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits



## STONEY CREEK GREENWAY SECTION 3

**Trail Distance:** 0.76 miles/4,013 feet

**Trail Type/Surface:** Type III. Asphalt

### Overview

Section three begins at Ash Street and runs south through Stoney Creek Park (a portion of this trail has already been constructed) to Elm Street. A HAWK signal is recommended as the at-grade crossing of Ash Street which would provide a major connection along Stoney Creek connecting parks and neighborhoods. The paved park trail should continue south to Elm Street. A HAWK signal crossing is also recommended at Elm, east of Stoney Creek Parkway. There are multiple east-west connections that can be made for improved neighborhood access to the park and trail such as E. Evergreen Avenue and E. Pine Street.

### Planning-Level Cost Estimate

The cost for trail construction only (based on an average cost of \$500K/mile) comes to \$200,000 given that portions of this trail have been completed. Other major costs related to this project would include the HAWK signals (\$125,000 per signal).

### Recommended Next Steps

- Secure funding to pave this section as the majority of the easements are already in place.
- Provide neighborhood links to the greenway trail from the dead end roads that exist west to east along this portion of trail.
- Secure any necessary easements that may be needed to provide an at grade crossing (HAWK signal) at Elm Street near Stoney Creek Parkway.
- Coordinate with Public Works regarding any sewer line work in this section.



*Existing Conditions*

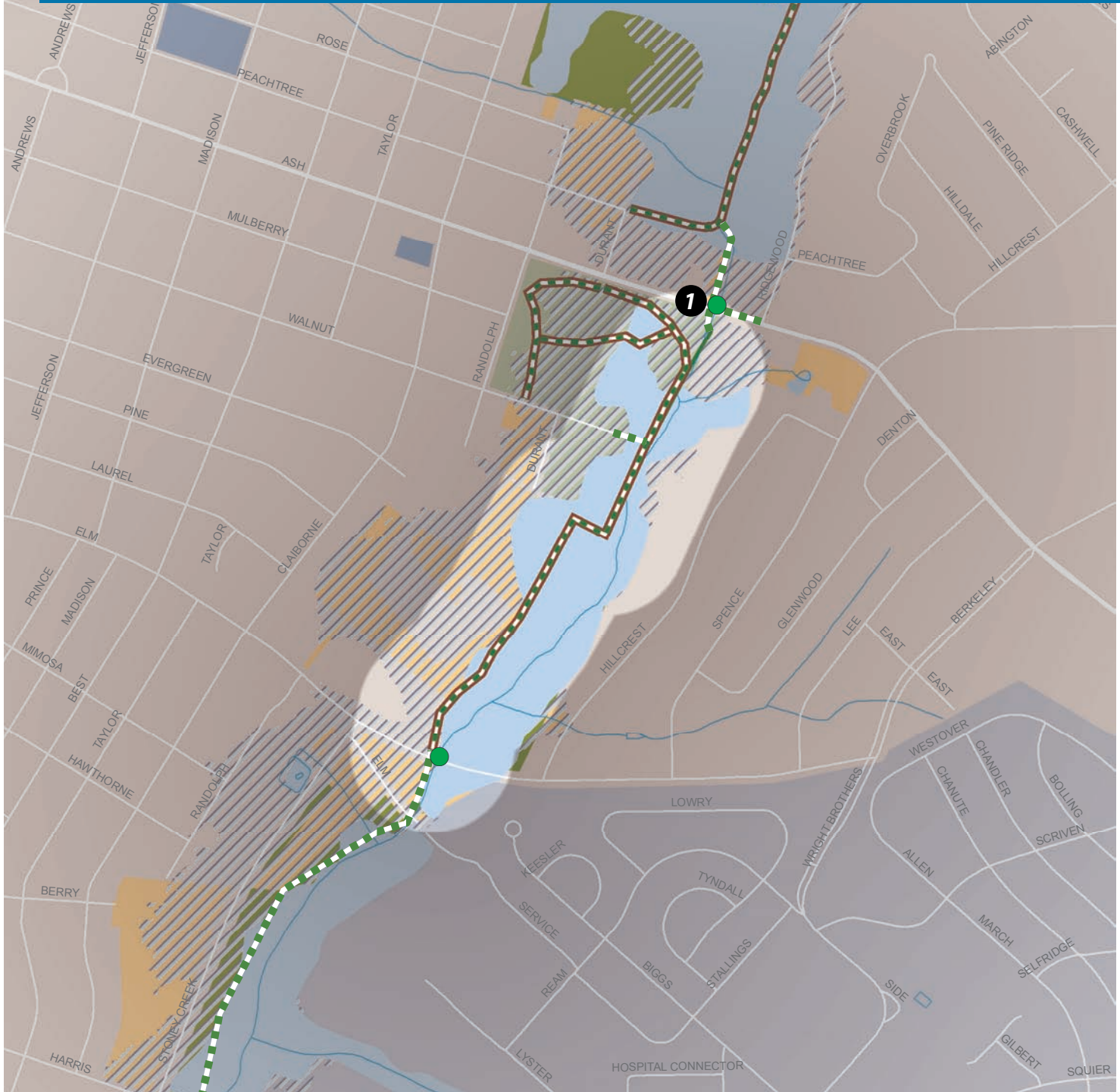
*The recommended HAWK signal at Ash Street would provide a well-defined and safe crossing for the Stoney Creek Greenway*



*Proposed*



# MAP 3.5 STONEY CREEK GREENWAY SECTION 3



**Stoney Creek Greenway - Section 3**



**Legend**

**Recommended Greenways**

- Shared-Use Path
- HAWK Signal

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits



## STONEY CREEK GREENWAY SECTION 4

**Trail Distance:** 1.71 miles/9,029 feet

**Trail Type/Surface:** Type III. Asphalt and/or concrete depending on floodway/floodplain and subsurface conditions.

### Overview

Section 4 Starts at Elm Street which will require an at-grade crossing (HAWK signal mentioned in Section 3 Overview). The proposed corridor continues south to Slocumb Street along publicly-owned land and sewer easement. The City owns a tract of land at Elm and Stoney Creek and it is recommended this be utilized as a trail head for the Stoney Creek Greenway. This section was previously designated as a Mountains-to -Sea Trail (MST) route and remains a priority. It will connect several neighborhoods, has historic tobacco barn structures west of the trail, and falls almost entirely within designated utility easements. Additionally, this section is slated for sewer improvements. It is recommended this improvement project partners to obtain access easements where needed and potentially include trail improvements should funding allow. This portion of the trail can help provide a connection between to entrances to Seymour Johnston Air Force Base, Public Housing neighborhoods, and parks.

### Planning-Level Cost Estimate

The cost for trail construction only (based on an average cost of \$500K/mile) comes to \$855,000. Other major costs related to this project could include trailhead development and any sections of trail that may require boardwalk.

### Recommended Next Steps

- Coordinate with Public Works on access easements and upcoming trail sewer line projects.
- Work on HAWK signal at Elm and begin planning for trail head at this location.

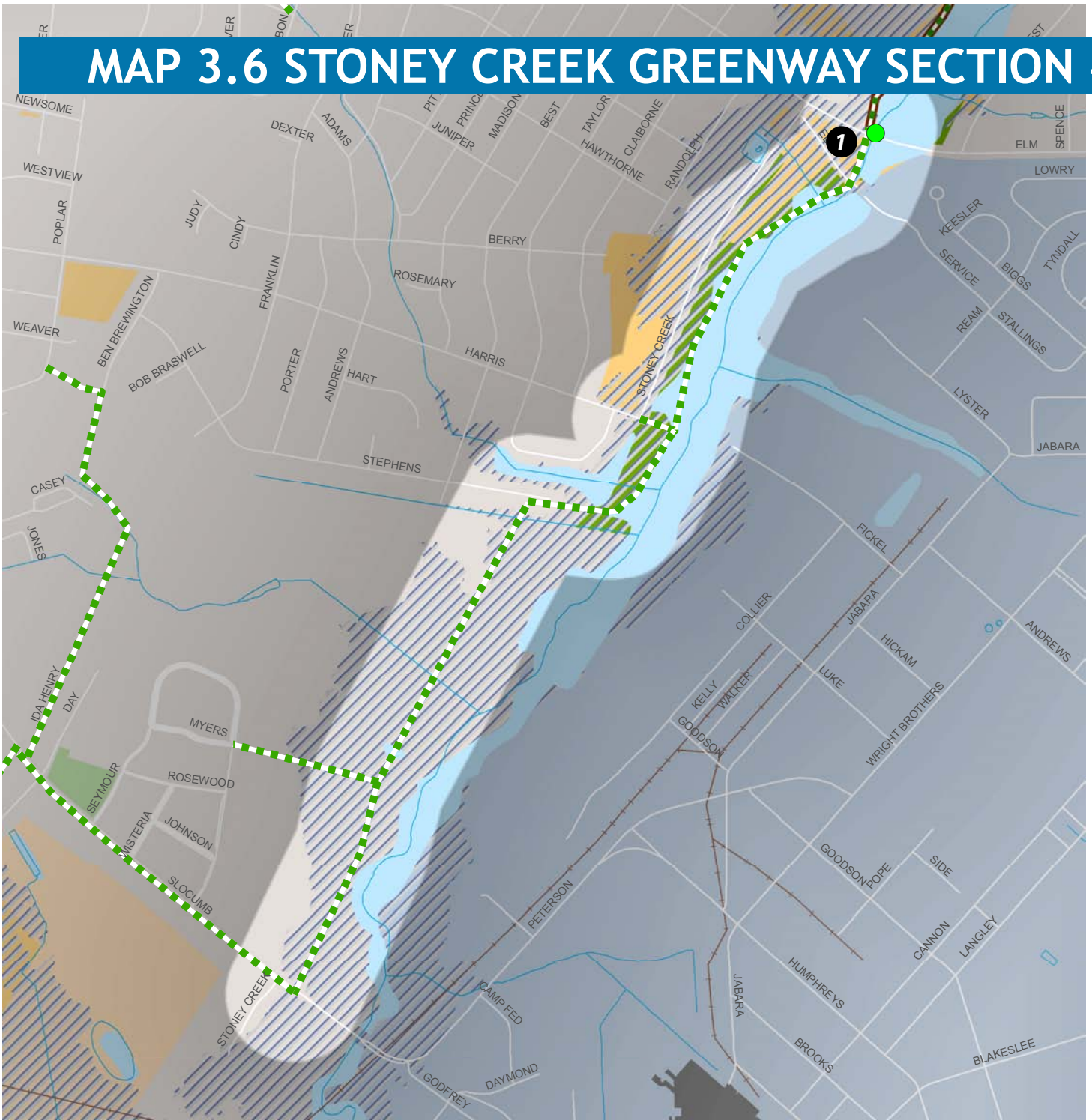


*The City-owned parcel on south side of Elm is an ideal location for the trailhead. The HAWK signal would be located near this location across Elm Street.*

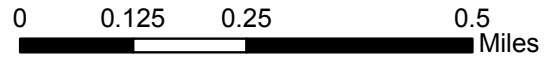




# MAP 3.6 STONEY CREEK GREENWAY SECTION 4



**Stoney Creek Greenway - Section 4**



**Legend**

**Recommended Greenways**

- Shared-Use Path
- HAWK Signal

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits





## STONEY CREEK GREENWAY SECTION 5

**Trail Distance:** 2.16 miles/11,405 feet

**Trail Type/Surface:** Type III. This section of trail will be asphalt surface using a variety of trail types including multi-use trail and sidepath.

### Overview

The last section is critical as the final link to the future Statewide Mountains-to-Sea Trail (MST) to be located along the Neuse River. There are several opportunities to connect to the future MST using publicly owned land along Slocumb, and continuing south along Westbrook Road. The City-constructed wetland and Cherry Farm property located along Westbrook can serve as environmental education opportunities for users.

### Planning-Level Cost Estimate

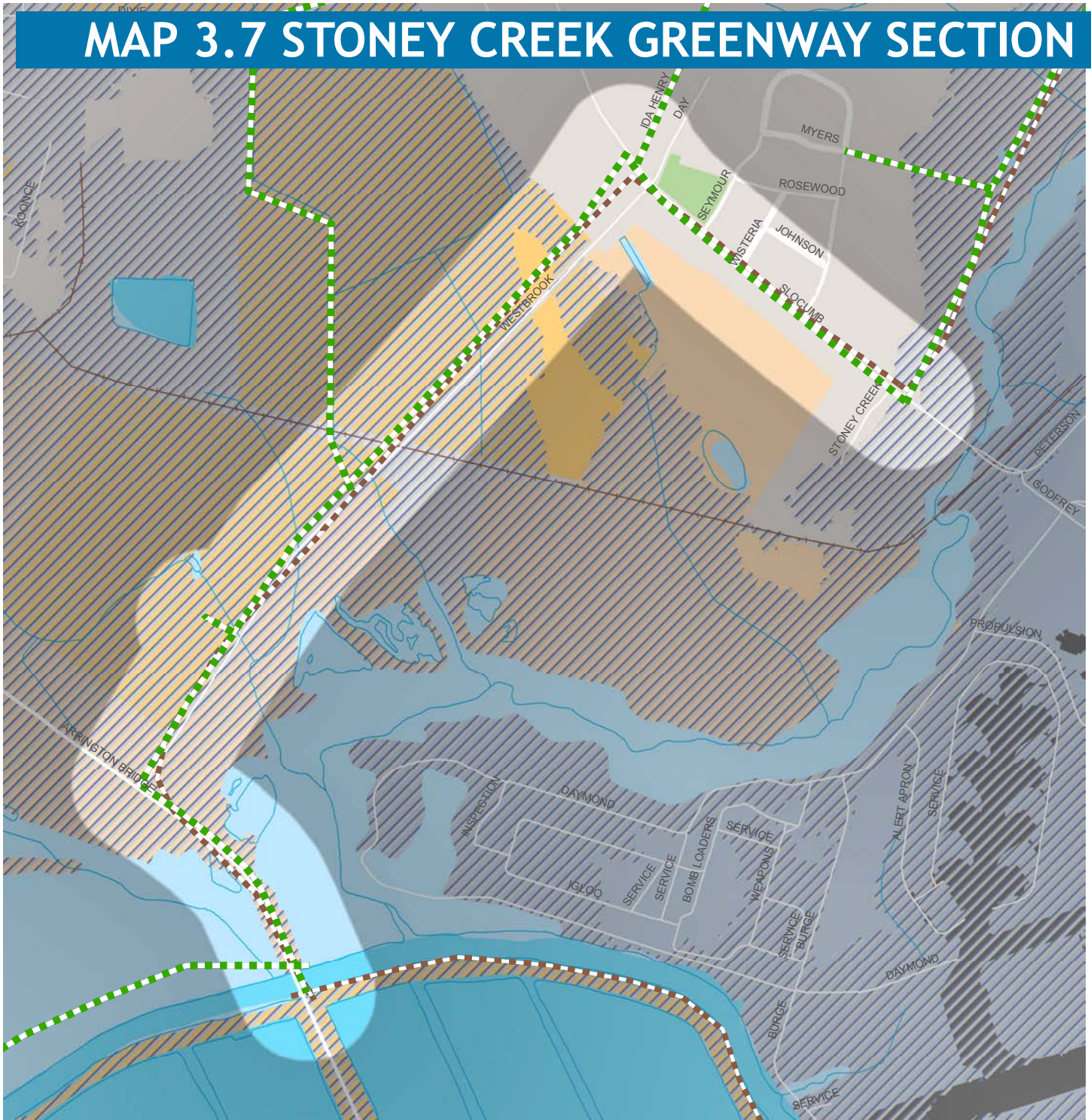
The cost for trail construction only (based on an average cost of \$500K/mile) comes to \$1.08 million. Other major costs related to this project could include some easement acquisition and greenway crossing of Slocumb.

### Recommended Next Steps

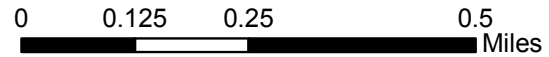
- Obtain easements and determine ROW for sidepath to be located along Slocumb.
- Provide pedestrian crossing improvements at Slocumb and Westbrook.
- Obtain easements and determine ROW for sidepath to be located along Westbrook.
- Provide educational signage and interpretive opportunities at the Cherry Farm site and Constructed Wetland.



# MAP 3.7 STONEY CREEK GREENWAY SECTION 5



Stoney Creek Greenway - Section 5



**Legend**

**Recommended Greenways**  
 ■ Shared-Use Path

**Existing Infrastructure**  
 ■ Existing Bike Lane  
 ■ Existing Shared-Use Path  
 ■ Funded Shared-Use Path  
 ■ Roadway  
 ■ Railroad

**Lands of Interest**  
 ■ City Greenway Lands  
 ■ Park  
 ■ Schools  
 ■ Hospital  
 ■ Other Destinations  
 ■ Other City/County Property

**Additional Context**  
 ■ Proposed Mountains-to-Sea Trail Alignment  
 ■ Water Features  
 ■ Wetlands  
 ■ 100-Year Floodplain  
 ■ Seymour Johnson AFB  
 ■ City Limits  
 ■ MPO Limits



## THE “DITCH” GREENWAY SECTION 1

**Trail Distance:** 1.11 miles/5,860 feet

**Trail Type/Surface:** Type III. This section of trail will be asphalt surface using a variety trail types including multi-use trail and possibly on-road sections.

### Overview

The “Ditch” is an existing drainage ditch that runs north-south just east of the core Goldsboro Downtown. A portion of the ditch remains paved and controlled with stormwater conveyed through a concrete channel; a portion of ditch to the south of Elm Street has been restored through Clean Water Management Trust Fund dollars (CWMTF), and another remains open natural channel and not restored. The majority of this channel falls along a sewer easement, public property, or within public housing and can serve as an important, practical, greenway connection. Section One includes the “Ditch” from Peacock Park located along Stronach Avenue south to Ash Street. This section remains an open unimproved channel with opportunity to provide greenway trail in partnership with stream improvements.

### Planning-Level Cost Estimate

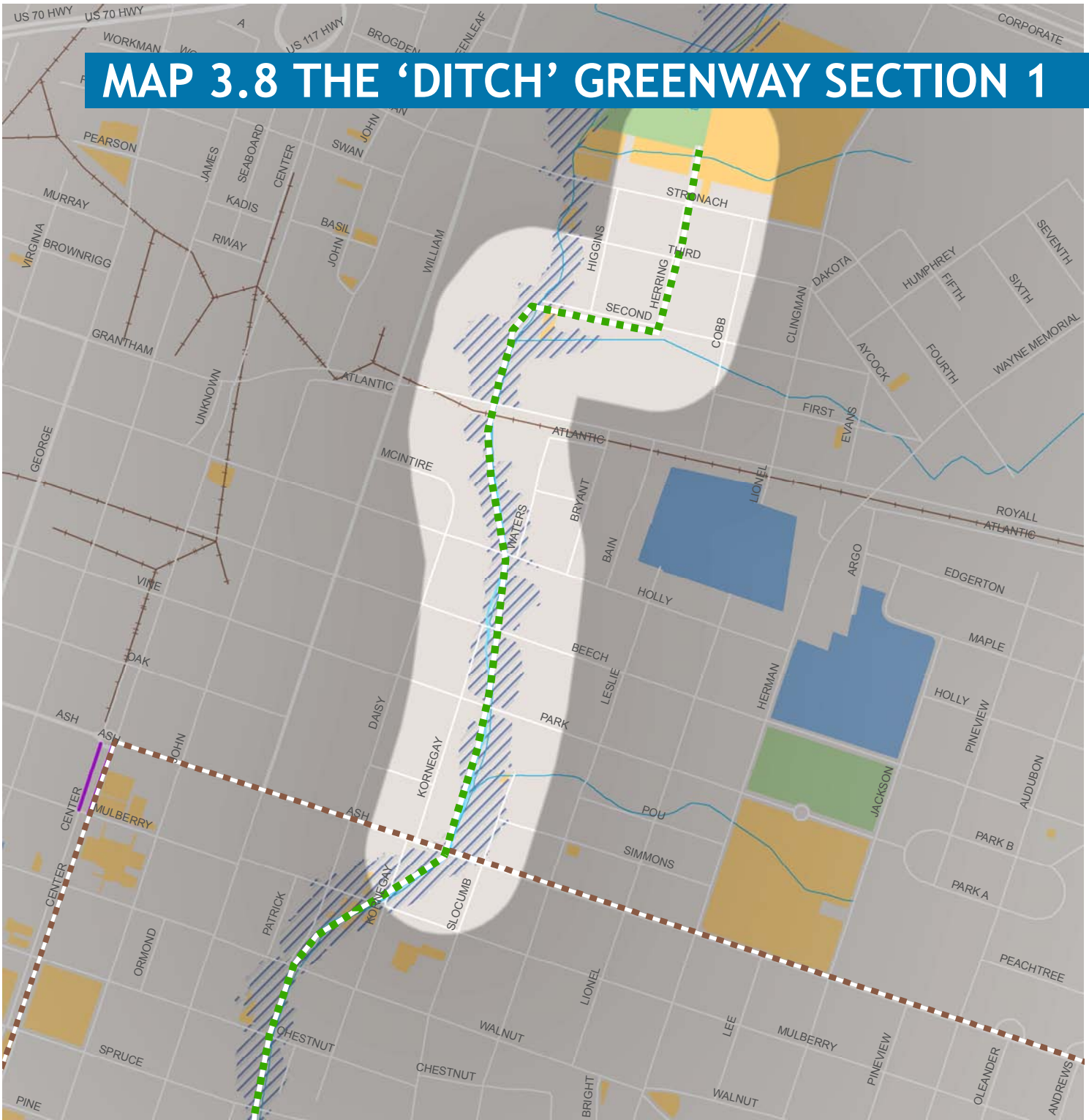
The cost for trail construction only (based on an average cost of \$500K/mile) comes to \$550,000. Other major costs related to this project could include roadway crossing improvements, some minor acquisition, and stream restoration efforts.

### Recommended Next Steps

- Examine interdepartmental partnerships for: Stream restoration, Sewer line and utility repair, and greenway trail.
- Obtain any access easements where needed.
- Study best methods for crossing east-west roadways including pedestrian crossing improvements at Royall.
- Determine portions of trail that may need to utilize roadway for one or multiple blocks due to constraints or space issues directly along creek.



*Portion of the “Ditch” with concrete channel through a public housing area at Elm Street.*



The "Ditch" Greenway - Section 1



**Legend**

**Recommended Greenways**

- Shared-Use Path

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Proposed Mountains-to-Sea Trail Alignment
- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits



## THE “DITCH” GREENWAY SECTION 2

**Trail Distance:** 1.65 miles/8,712 feet

**Trail Type/Surface:** Type III and II. This section of trail will be asphalt surface through cemetery and possibly crushed gravel (Type II) west of cemetery.

### Overview

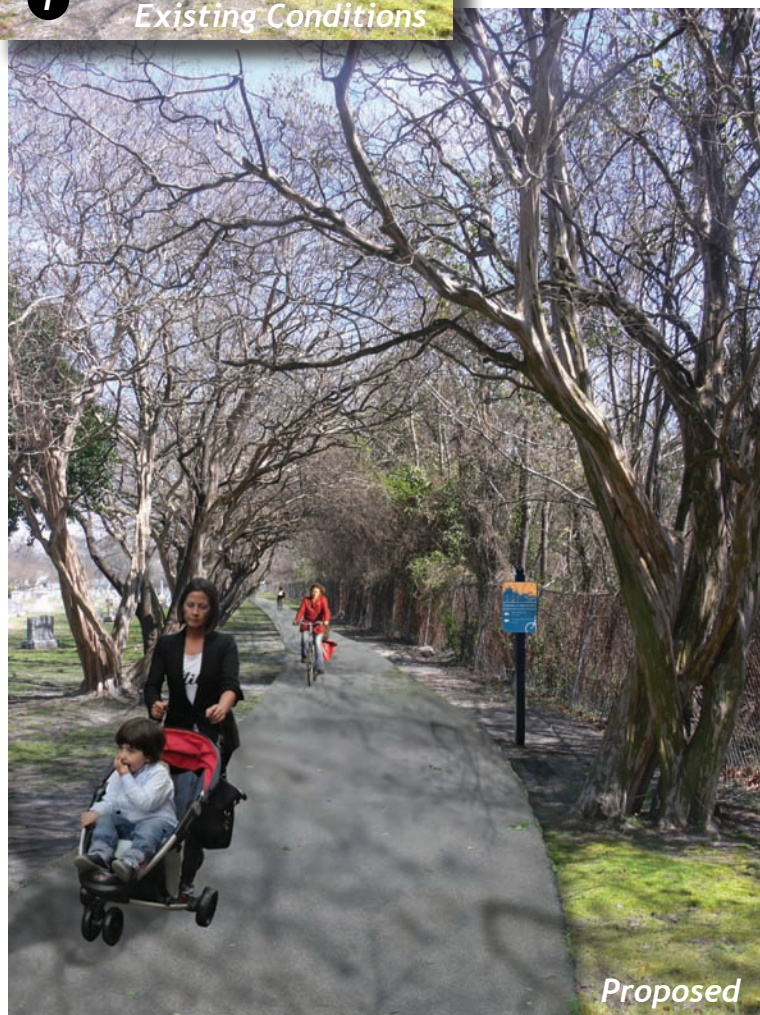
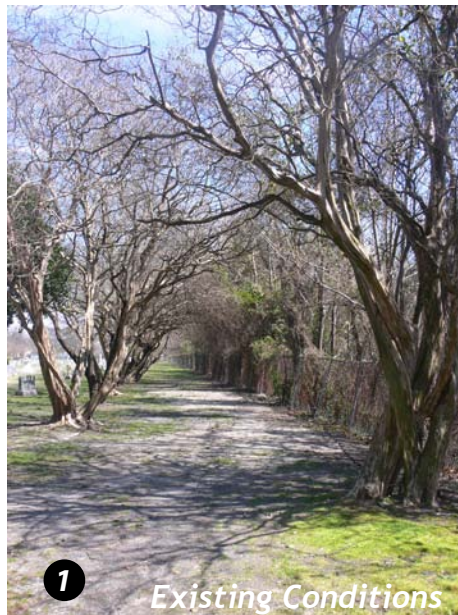
This section of trail runs from Ash Street south to Highway 117 with several opportunities to provide park improvements and greenway trails connecting neighborhoods, commercial areas, public housing, and parks. The ditch is channelized from Ash to Elm and is situated along roads, behind homes, and within public housing communities. There is an opportunity to connect along the channel and to study opportunities for restoration of the channel to a natural state, in partnership with providing public access. From Elm to Highway 117, the channel has been partially restored and has adjacent open space perfect for providing greenway trail. There is an adjacent cemetery that could be utilized for historic interpretation and two FEMA buyout neighborhoods with the potential for public park space (one site is currently being used as a community garden). In addition, a greenway spur is recommended from the cemetery to Mina Weil Park and Dillard Middle School utilizing Sycamore Street and possibly requiring an easement.

### Planning-Level Cost Estimate

The cost for trail construction only (based on an average cost of \$500K/mile) comes to \$825,000.

### Recommended Next Steps

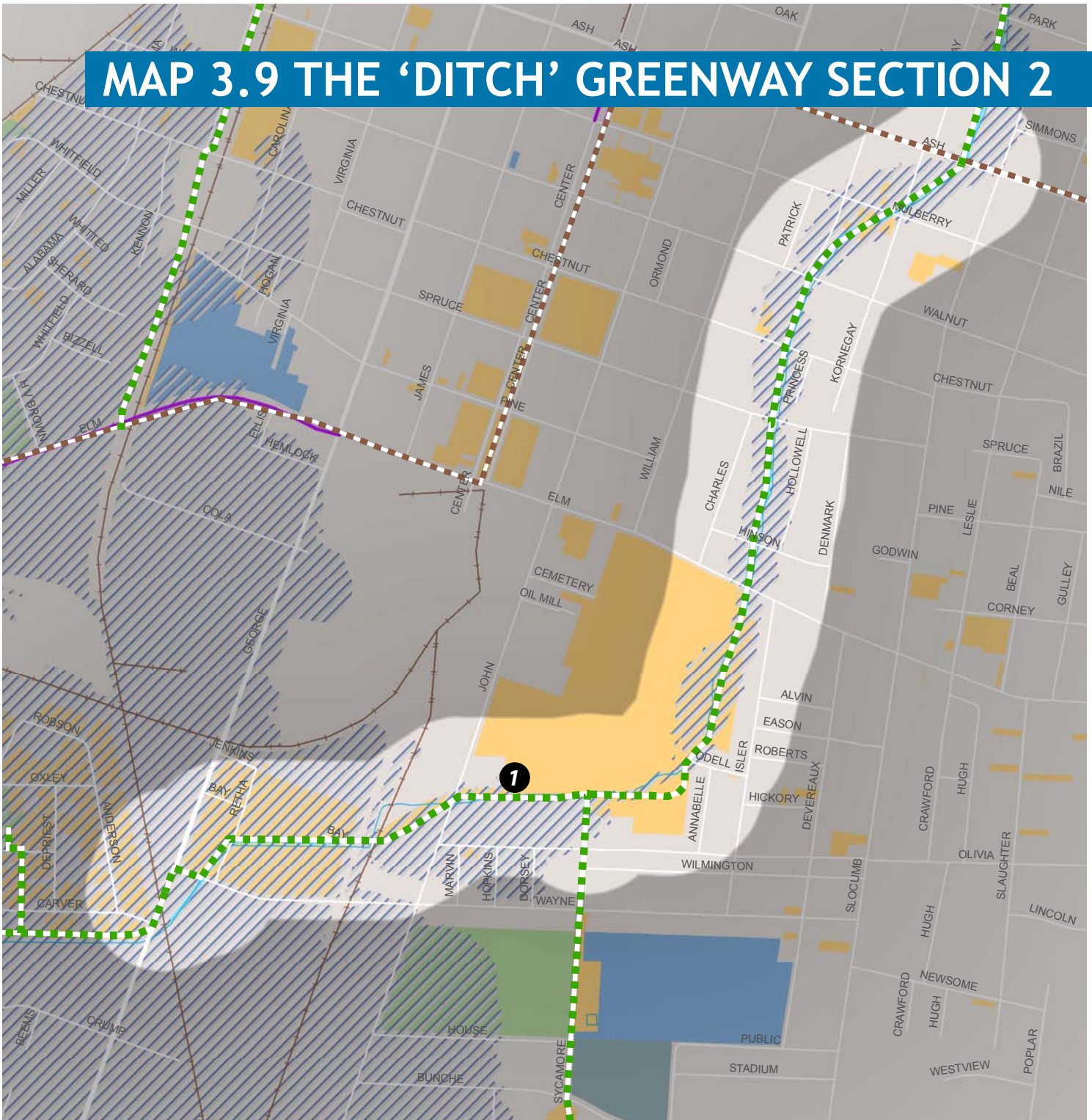
- Create a detailed map of existing public easements and public land.
- Evaluate opportunities for stream restoration and grants for completing these studies and restoration work.
- Evaluate land use potential for current publicly owned land from Elm to Highway 117.
- Determine appropriate crossing facility for crossing George and John.



*The archway of crape myrtles provides an opportunity for a scenic stretch of this greenway on the south side of the cemetery.*



# MAP 3.9 THE 'DITCH' GREENWAY SECTION 2



The "Ditch" Greenway - Section 2



**Legend**

**Recommended Greenways**

- Shared-Use Path

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Proposed Mountains-to-Sea Trail Alignment
- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits



## MOUNTAINS-TO-SEA TRAIL SECTION 1

**Trail Distance:** 9.95 miles/52,536 feet

**Trail Type/Surface:** Type I and II.

### Overview

This Statewide trail can become a valuable tourism asset to Goldsboro. Goldsboro is fortunate to have several boating access locations along the Neuse, as well as publicly owned land that can serve as future blueway support. These features, along with connectivity to Johnston County's Bentonville Battlefield, will help connect natural, cultural, and historic resources. The first phase would leave Johnston County on the south side of the Neuse and the Bentonville Battlefield and continue east along the Neuse through Duke Energy property, with the opportunity to link directly into Goldsboro via Waynesborough Historic Village near Highway 117. East of Section 1, the trail would continue east into Goldsboro via on-road bicycle and sidewalk facilities. From Quaker Neck Lake, MST users would have the option of taking a trail spur south and east along the Neuse River.

### Planning-Level Cost Estimate

The cost for trail construction only (based on an average cost of \$100K/mile) comes to \$995,000. Other major costs related to this project would include easement acquisition, bridge crossings, and any sections where boardwalk may be necessary. ***If built on the southern side, the number of privately owned properties it crosses is 76 (six are public).***

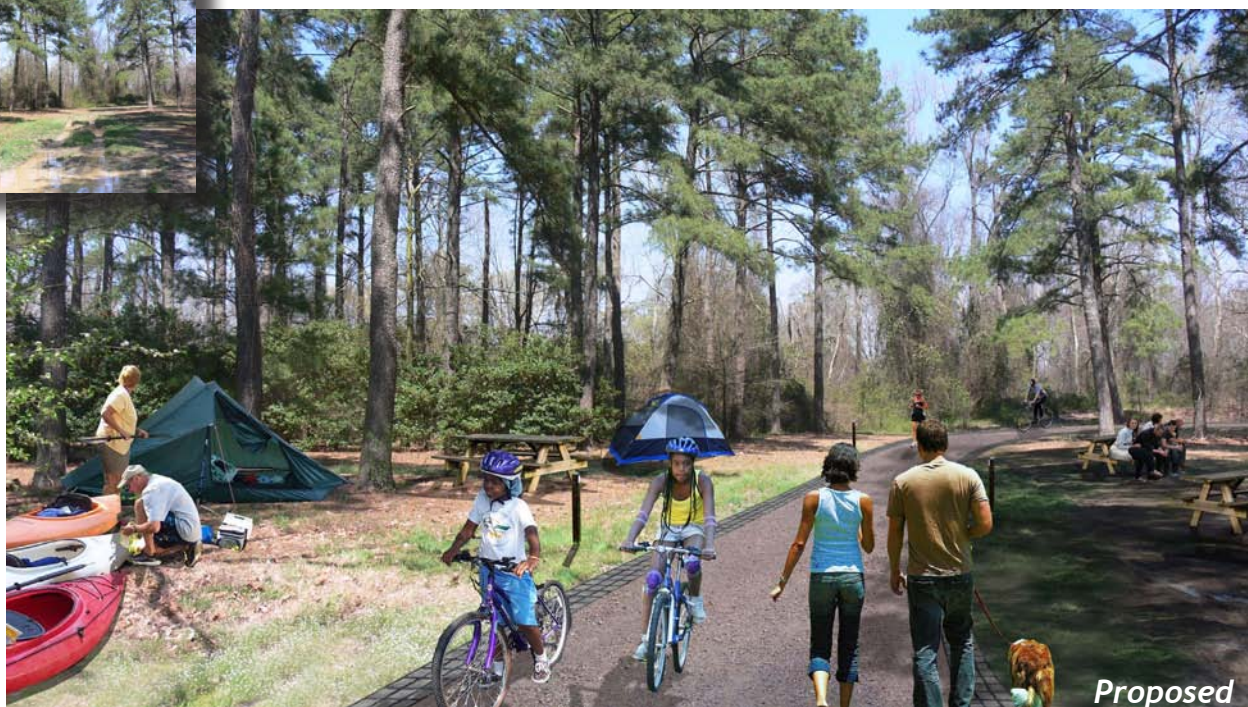
### Recommended Next Steps

- Begin a detailed landowner study for acquisition of necessary easements for trail completion.
- Coordinate with large landowners including Duke Energy and the State of North Carolina near Cherry Hospital to begin easement acquisition.
- Look for funding partners for easement acquisition, design, and construction of this section of trail.

#### Existing Conditions



*The FEMA floodplain buyout property near the Neuse River offers a tremendous opportunity for the Mountains-to-Sea Trail along with a campground and full canoe/kayak launch site.*

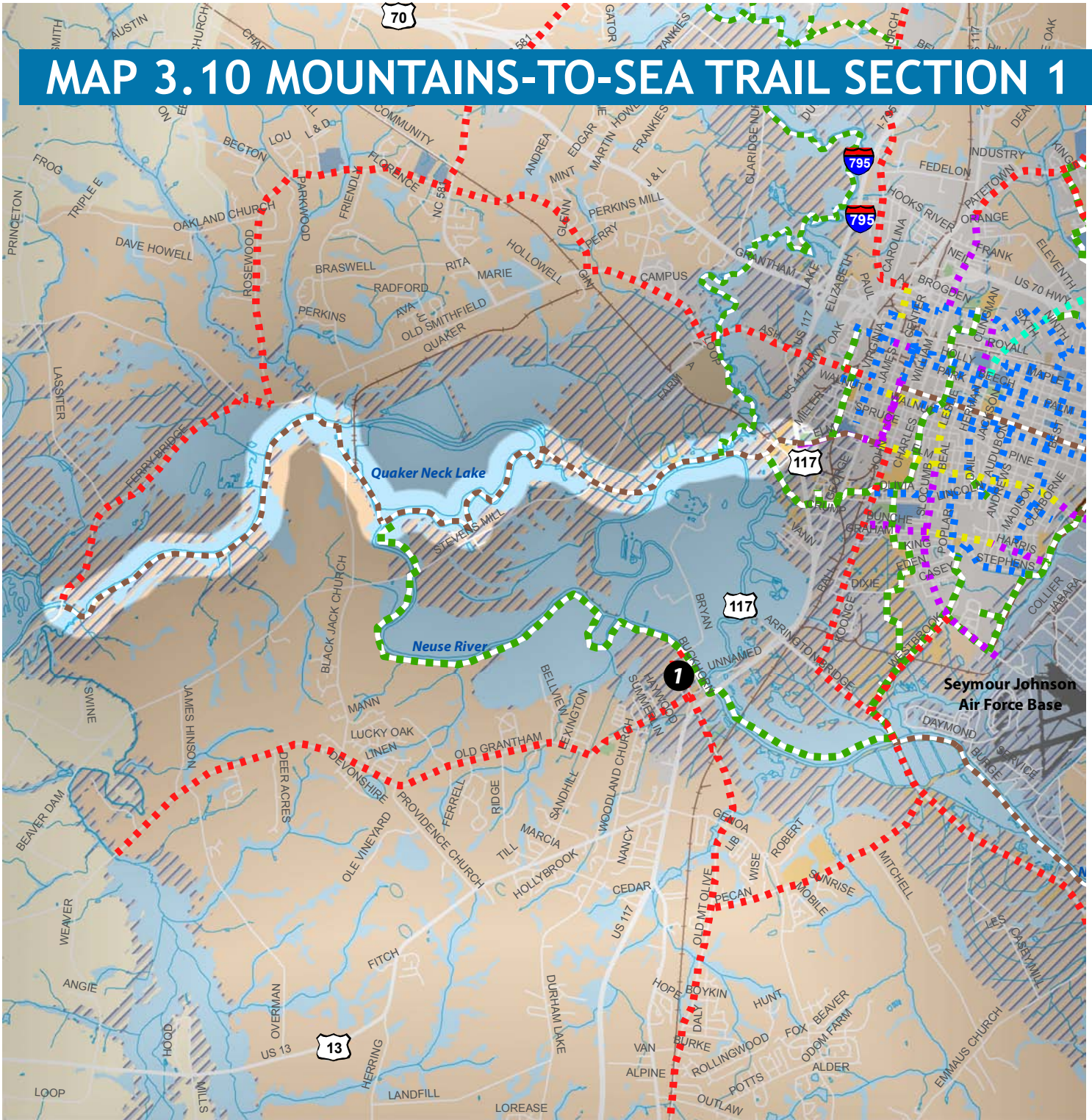


*Proposed*





# MAP 3.10 MOUNTAINS-TO-SEA TRAIL SECTION 1



**Mountains to Sea Trail - Section 1**



**Legend**

**Recommended Greenways**

- Bike Lane
- Bike Boulevard
- Paved Shoulder
- Shared Lane Marking (Sharrow)
- Bike Route Signage
- Wide Outside Lane
- Shared-Use Path

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Proposed Mountains-to-Sea Trail Alignment
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits



## MOUNTAINS-TO-SEA TRAIL - SECTION 2

**Trail Distance:** 8.43 miles (4.9 miles shared-use path; 3.5 miles on-road)

**Trail Type/Surface:** Type III and on-road facilities

### Overview

This section of the MST would connect Waynesborough Village into Downtown Goldsboro and eventually the Stoney Creek Greenway via a combination of on-road bicycle facilities, sidewalks, and shared-use paths. The trail would utilize newly-striped buffered bike lanes on Elm Street from US 117 to George Street, then continue along Elm Street to Center. The alignment would follow Center Street north into the Downtown streetscape which will include bike lanes. The route would then turn east on Ash Street to Stoney Creek Park. The Mulberry Street Bike Boulevard would provide a parallel route for bicyclists who do not wish to ride on Ash Street. From Ash Street, the spur would utilize the Stoney Creek Greenway and sidepaths southward to the Neuse River. This section would provide considerable benefit to Goldsboro by connecting visitors to downtown and linking Goldsboro residents to the MST.

### Planning-Level Cost Estimate

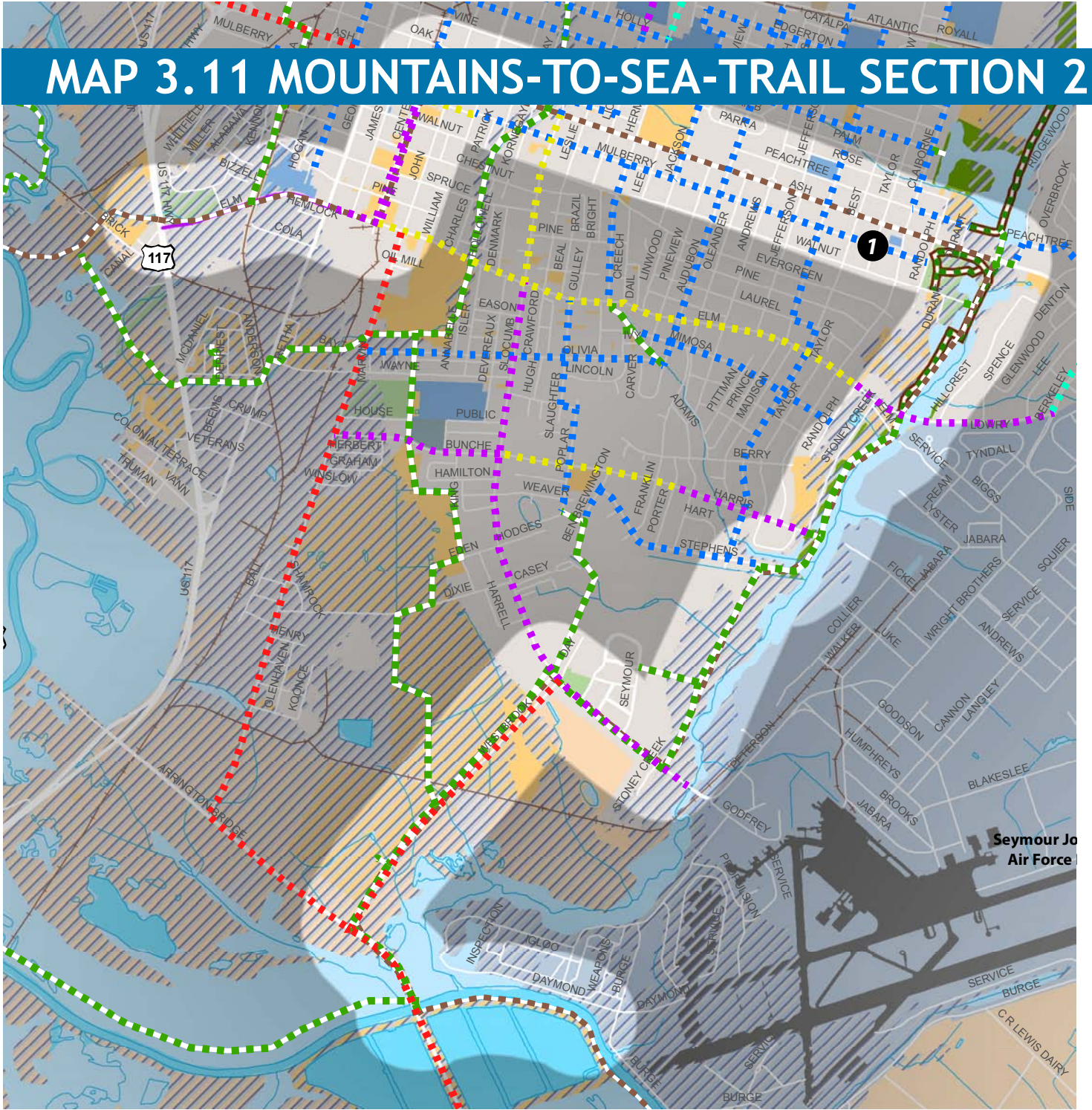
The cost for this section contains costs already outlined in other portions of the Plan (such as Stoney Creek Greenway enhancements).

### Recommended Next Steps

- Begin development of Mulberry Bike Boulevard.
- Stripe bike lanes on southern portions of Center Street.
- Evaluate US 117/Elm crossing for bicycle and pedestrian crossing treatments such as marked crosswalks and countdown signals.



*Mulberry Street is a recommended bike boulevard/neighborhood greenway (see Chapter 4 for more information). With the addition of traffic calming and landscaping, this would provide MST access for bicyclists.*



**Mountains to Sea Trail - Section 2**



**Legend**

**Recommended Greenways**

- Bike Lane
- Bike Boulevard
- Paved Shoulder
- Shared Lane Marking (Sharrow)
- Bike Route Signage
- Wide Outside Lane
- Shared-Use Path

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Proposed Mountains-to-Sea Trail Alignment
- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits



## MOUNTAINS-TO-SEA TRAIL SECTION 3

**Trail Distance:** 19.6 miles/103,488 feet

**Trail Type/Surface:** Type I and II.

### Overview

This section of the MST will continue east from the municipally owned land near the Neuse and Westbrook Road, along the south side of the Neuse, ultimately connecting to the Cliffs of the Neuse State Park and the Town of Seven Springs. Both of these destinations can serve as key support stops for blueway and greenway users along the Mountains-to Sea Trail corridor. The Cliffs of the Neuse offers camping, water access, bathrooms, additional hiking, freshwater lake facilities and host of other opportunities. Seven Springs has an existing boating access, outfitter store, convenience shop, and potential for future trail-supported business.

### Planning-Level Cost Estimate

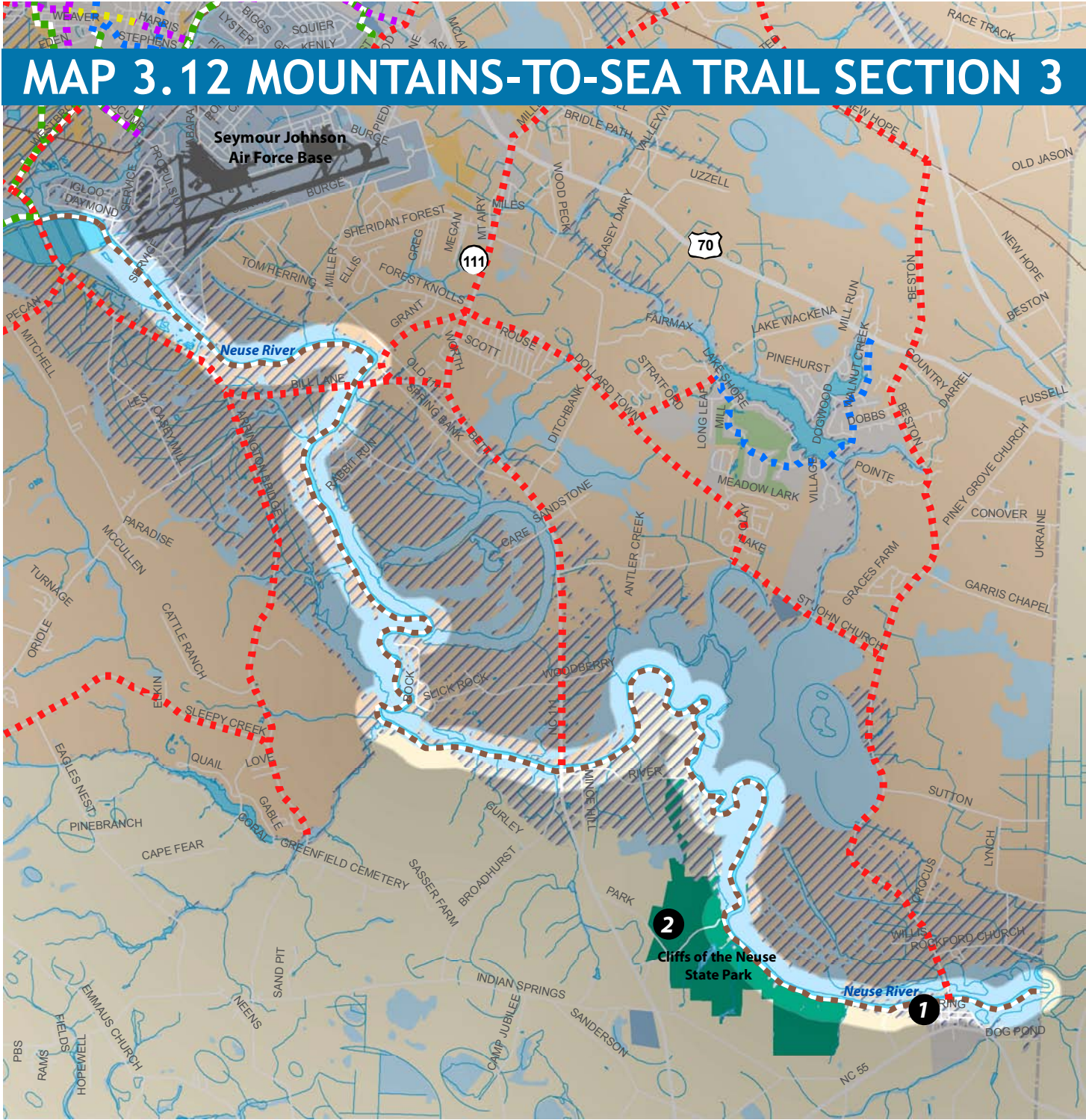
The cost for trail construction only (based on an average cost of \$100K/mile) comes to \$1.96 million. Other major costs related to this project would include easement acquisition. ***If built on the southern side, the number of privately owned properties it crosses is 177 (16 of which are publicly-owned).***

### Recommended Next Steps

- Begin a detailed landowner study for trail easement acquisition.
- Coordinate with large landowners including the State of North Carolina and Cliffs of the Neuse State Park.
- Look for funding partners for easement acquisition, design, and construction of this section of trail.



During the planning process, residents and stakeholders indicated a strong desire to connect Goldsboro to Cliffs of the Neuse State Park (top) and Seven Springs (left).



### Mountains to Sea Trail - Section 3



**Legend**

**Recommended Greenways**

- Bike Lane
- Bike Boulevard
- Paved Shoulder
- Shared Lane Marking (Sharrow)
- Bike Route Signage
- Wide Outside Lane
- Shared-Use Path

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Proposed Mountains-to-Sea Trail Alignment
- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits





## LITTLE RIVER TRAIL SECTION 1

**Trail Distance:** 8.4 miles/44,300 feet (additional spurs on northern end total 1.6 miles)

**Trail Type/Surface:** Type II. This section of trail will be predominantly natural surface/crushed gravel with the potential of boardwalk in places through wetter locations. The material surface will be dictated by the floodway/floodplain and subsurface conditions.

### Overview

Section One is the southernmost section of the proposed Little River Trail and is located along Little River; it begins at the Neuse River confluence and continues north to the Fallingbrook and Ashby Hills subdivisions off Buck Swamp Road. It crosses three highways (W. Ash, US 70, and future US 70 Bypass) with opportunity for underpasses in each situation. This is an important connection to connect the City of Goldsboro with neighborhoods to the northwest of town. This section of trail would only require easements along two properties, with an additional three properties to access Buck Swamp Road neighborhoods via trail spur.

### Planning-Level Cost Estimate

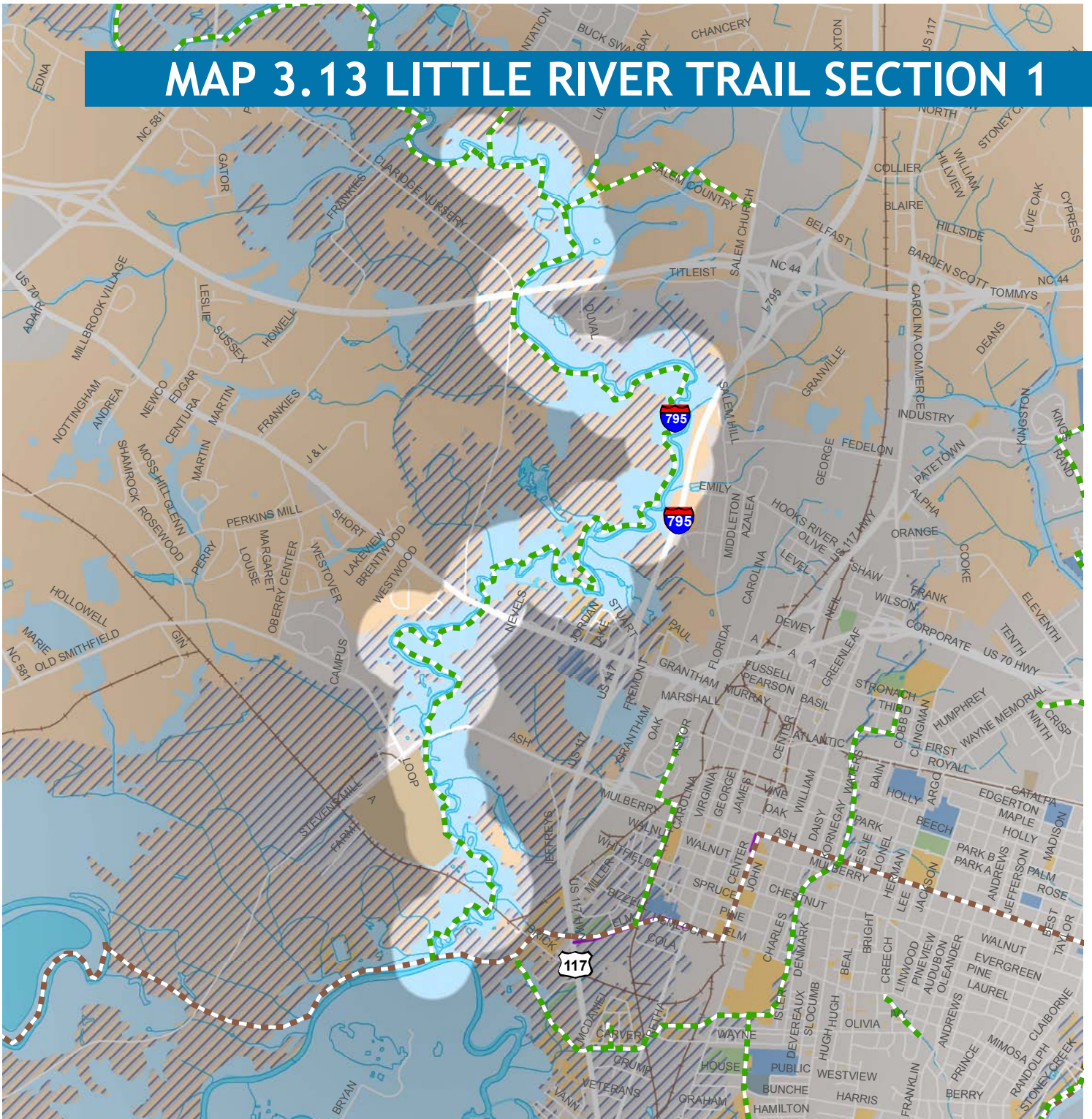
The cost for trail construction only (based on an average cost of \$100K/mile (for unpaved plus two river bridges) comes to \$1.34 million. Other major costs related to this project would include underpass improvements along with property acquisition.

### Recommended Next Steps

- Evaluate roadway crossings for underpass potential.
- Evaluate railroad crossing at southern end of this corridor for underpass opportunity.
- Begin the process of identifying properties where easements are needed.



# MAP 3.13 LITTLE RIVER TRAIL SECTION 1



Little River Greenway - Section 1



**Legend**

**Recommended Greenways**

- Shared-Use Path

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Proposed Mountains-to-Sea Trail Alignment
- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits





## LITTLE RIVER TRAIL SECTION 2

**Trail Distance:** 11.3 miles/59,800 feet

**Trail Type/Surface:** Trail Type/Surface: Type I and II.

### Overview

This section of Trail runs from the end of Section 1 (Fallingbrook and Ashby Hills subdivisions off Buck Swamp Road) to the Wayne County and project study area border. This is a longer term project that would connect to greenway planning efforts ongoing in Johnston County. A total of approximately 50 parcels were counted during this study where trail easements would be required.

### Planning-Level Cost Estimate

The cost for trail construction only (based on an average cost of \$100K/mile (for unpaved) comes to \$1.13million. Other major costs related to this project will include acquisition costs.

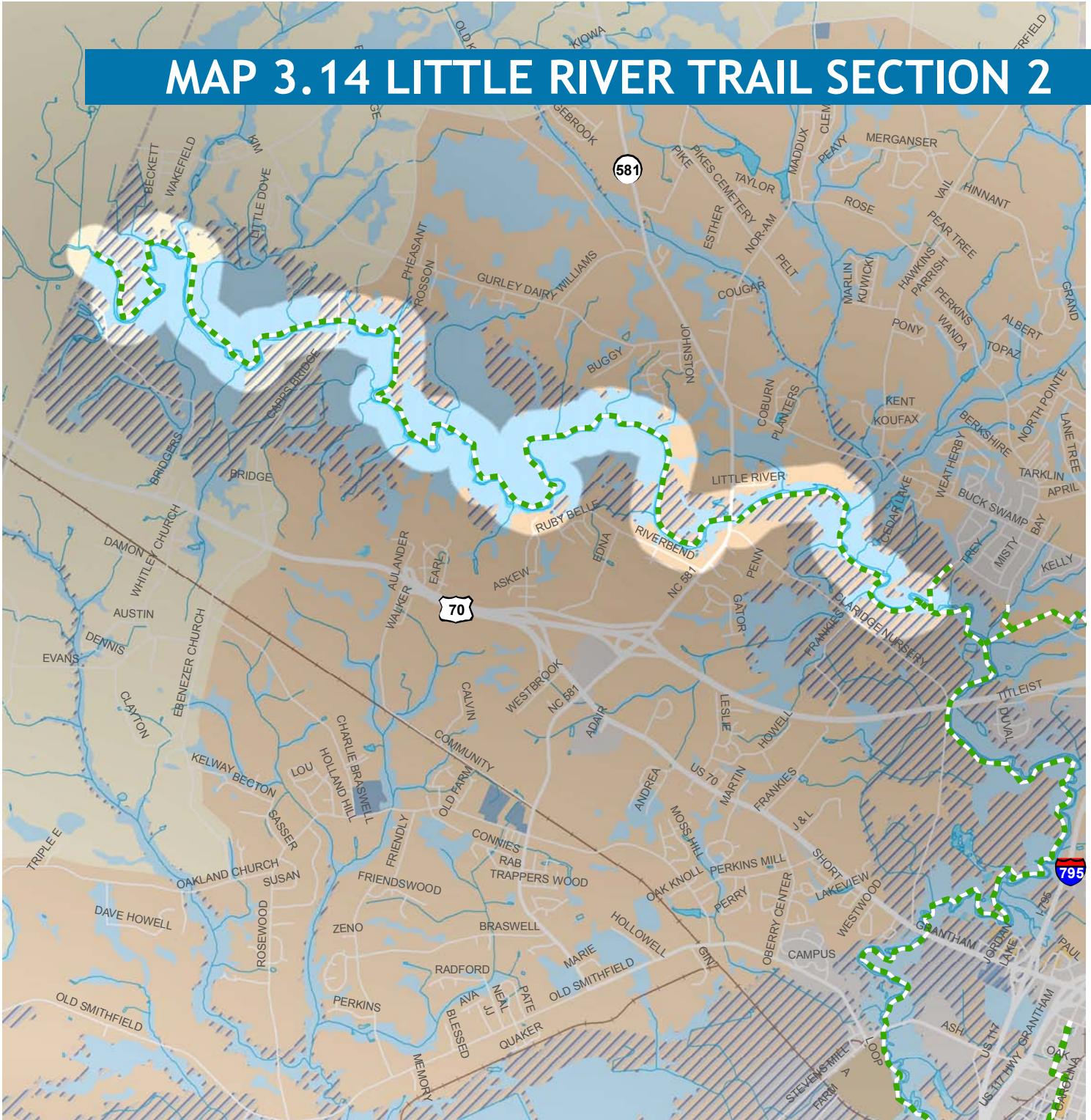
### Recommended Next Steps (Long-term)

- Create a detailed map of existing public easements and public land.





# MAP 3.14 LITTLE RIVER TRAIL SECTION 2



Little River Greenway - Section 2



**Legend**

**Recommended Greenways**

- Shared-Use Path

**Existing Infrastructure**

- Existing Bike Lane
- Existing Shared-Use Path
- Funded Shared-Use Path
- Roadway
- Railroad

**Lands of Interest**

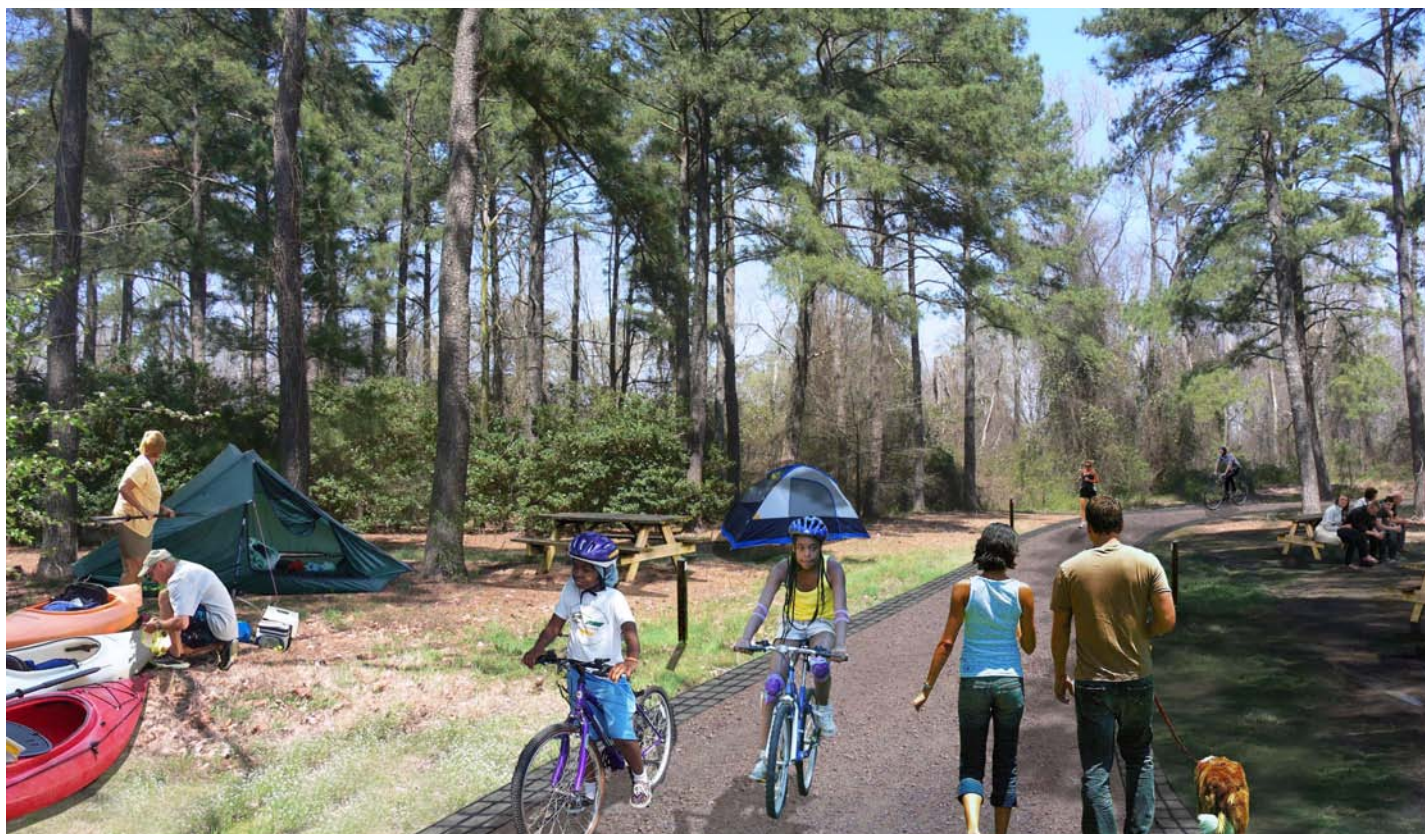
- City Greenway Lands
- Park
- Schools
- Hospital
- Other Destinations
- Other City/County Property

**Additional Context**

- Water Features
- Wetlands
- 100-Year Floodplain
- Seymour Johnson AFB
- City Limits
- MPO Limits



## GOLDSBORO PHOTO VISUALIZATION: MOUNTAINS-TO-SEA TRAIL AT BRYAN BLVD AND NEUSE



*FEMA floodplain buyout property near the Neuse River offers a tremendous opportunity for the Mountains-to-Sea Trail along with a campground and full canoe/kayak launch site.*