



Appendix I: Demonstration Projects

Chapter Contents:

Overview

Project List

*Demonstration Project
Cutsheets*

OVERVIEW

This appendix includes advanced concepts and planning-level cost estimates for a selection of the highest priority on-road pedestrian and bicycle projects including a visionary, long-term recommendation for Ash Street road diet. These projects address some of the greatest need in the Goldsboro MPO and many have already been submitted to NCDOT as priorities for funding consideration.

PROJECT LIST

Goldsboro

- Royall Avenue Sidewalk #1
- Royall Avenue Sidewalk #2
- Spence Avenue Bicycle and Pedestrian Facilities
- Berkeley Boulevard Bicycle and Pedestrian Facilities #1
- Berkeley Boulevard Bicycle and Pedestrian Facilities #2
- Elm Street Bicycle and Pedestrian Improvements
- Herman Street Sidewalks
- Harris Street/Bunche Drive Bicycle and Pedestrian Facilities
- Ash Street Road Diet (Vision Project)
- Ash Street Sidewalk
- New Hope Road Sidepath
- Wayne Memorial Drive Sidewalks
- New Hope Road and Harding Road Intersection Improvements
- Mulberry Bike Boulevard
- Holly/Beech Bike Boulevard
- Audubon/Olivia Bike Boulevard

Walnut Creek

- Walnut Creek Drive Sidewalks
- Mill Road Sidewalks

Pikeville

- Main Street Bicycle and Pedestrian Facilities

CITY OF GOLDSBORO PROJECTS

G1: Royall Avenue Sidewalk #1

The section of Royall Avenue between Spence Avenue and Berkeley Boulevard is home to many different commercial and retail land uses. With the railroad paralleling the road along its south side, the north side is the only area that is serving adjacent development. No bicycle or pedestrian facilities currently exist along this road. The recommended treatment is to construct a sidewalk along the north side of Royall Avenue.

Extents and Facility Type: Spence Avenue to Berkeley Boulevard: Sidewalk on North Side

Overview and Purpose

Providing a sidewalk along the north side of Royall Avenue will create a pedestrian linkage to all the adjacent development. The sidewalk is recommended to be five to six feet wide, with a verge of at least three feet. To fit this within the existing right of way, the shoulder and ditch section on the north side will need to be converted to a curb and gutter section. Drainage improvements made in conjunction with this transition should help alleviate flooding problems experienced at the intersection of Royall Avenue and Spence Avenue. Intersection improvements are recommended at Royall Avenue and Spence Avenue, including high visibility crosswalks and pedestrian countdown signals. The right in-right out movement into the Exxon in the northeast quadrant of the Royall Avenue and Spence Avenue intersection should include an improved “pork chop” area with a sidewalk pulled away from the travel lane. These improvements will interface with the recommended sidepath along the west side of Spence Avenue, discussed further as project G3. The exhibit shows a detail of the intersection of Royall Avenue and Spence Avenue, indicating the positioning of the recommended sidewalk, supporting road crossing improvements, and the linkage with the proposed sidepath along Spence Avenue.

Planning Level Cost Estimate: \$330,000





G2: Royall Avenue Sidewalk #2

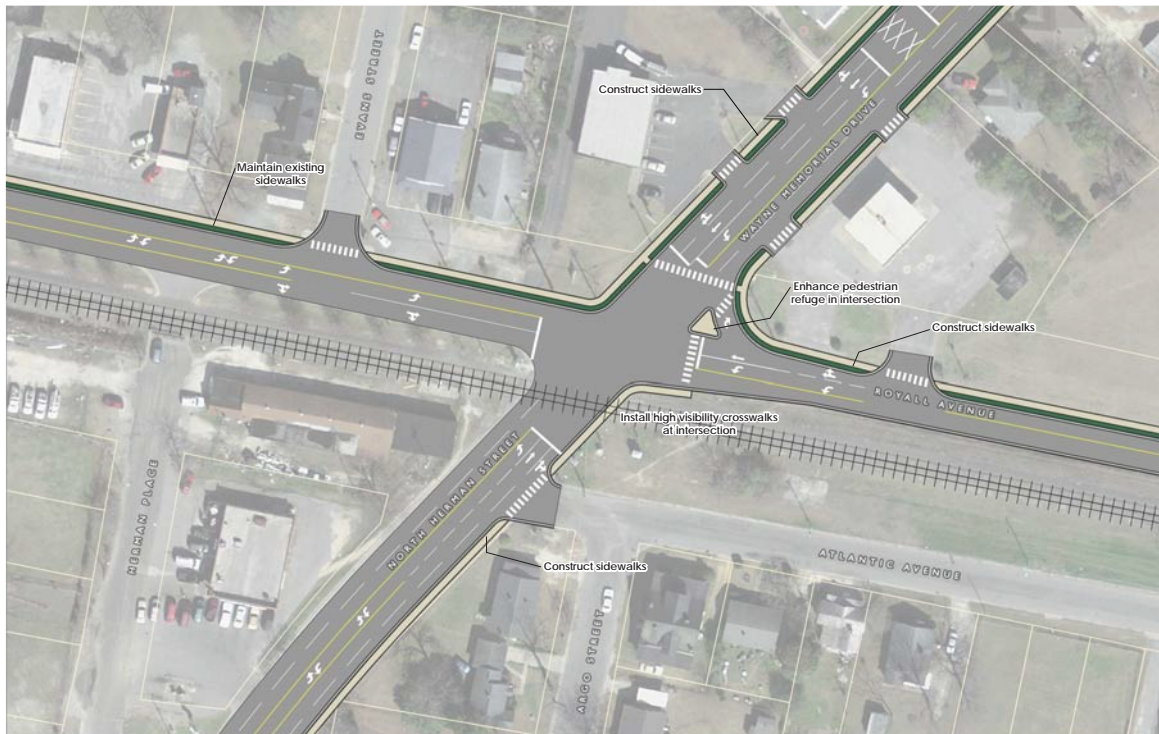
Between Spence Avenue and William Street, Royall Avenue contains a mixture of commercial and residential land uses. The railroad continues to parallel the road on its south side for the entire extents of this project, resulting in Royall predominantly serving development on its north side. There are currently no bicycle facilities along this roadway, and only limited sidewalk connections. The recommended treatment is to construct a sidewalk along the north side of Royall Avenue.

Extents and Facility Type: William Street to Spence Avenue: Sidewalk on North Side

Overview and Purpose

Royall Avenue is a key east-west roadway linkage within the City of Goldsboro that will benefit from the provision of a consistent pedestrian facility. A five to six foot sidewalk is recommended on the north side of the street, with at least a three foot verge area. At the intersection of Royall Avenue and Wayne Memorial Drive, intersection crossing improvements will be needed to assist pedestrians. High-visibility crosswalks are recommended along the southbound and westbound approaches to line up with recommended sidewalks. The current “pork chop” area separating the through and right turning traffic on the westbound approach should be enhanced to provide a true pedestrian refuge. The crosswalk on the southbound approach should be lined up to match curb ramps that currently exist. The exhibit shows a detail of the intersection of Royall Avenue and Wayne Memorial Drive, indicating the positioning of the recommended sidewalk, supporting road crossing improvements, and the linkage with proposed sidewalks along Wayne Memorial Drive/North Herman Street. The exhibit also shows where the proposed sidewalk will cross the railroad tracks south of Royall Avenue. In this location, the sidewalk will run at grade with the roadway rather than being elevated on curb and gutter. Likely consisting of an extended asphalt section (rather than concrete), this treatment still provides a separate space for pedestrians without interfering with the operation of the rail line.

Planning Level Cost Estimate: \$840,000



G3: Spence Avenue Bicycle and Pedestrian Facilities

The section of Spence Avenue between Ash Street and the US 70 Bypass is a primarily commercial corridor, providing connections to many of the major retail destinations in the City of Goldsboro. As a result, the corridor contains many destinations that would be desirable for access by nonmotorized users. Spence Avenue transitions to Cuyler Best Road as a bridge over the US 70 Bypass. No bicycle or pedestrian facilities currently exist along this road. The recommended treatment is to construct a sidepath along the west side of Spence Avenue, along with restriping the roadway to accommodate wide outside lanes.

Extents and Facility Type: Ash Street to US 70 Bypass: Sidepath on West Side; Sidewalk on east side; Ash Street to US 70 Bypass: Wide Outside Lanes (Restripe)

Overview and Purpose

The sidepath recommended along the west side of Spence Avenue is recommended to be eight to ten feet wide, using the ten foot width wherever possible. In addition, a small verge section is recommended to separate this facility from the vehicle travelway. Laneage along Spence Avenue should be reconfigured to provide 14 foot outside lanes, 11 foot inside lanes, and a 10 foot center turn lane. The exhibit shows a detailed view of the section of Spence Avenue between Royall Avenue and the US 70 Bypass. In this section, high-visibility crosswalks are recommended to traverse driveways intersecting with the sidepath in order to provide a consistent travel experience. The character of the improvements change over the US 70 Bypass bridge. In this area, the bridge should be restriped to provide an eight foot shoulder on the west side and a four foot shoulder on the east side. The wider shoulder on the west side provides the alternative for people using the sidepath to continue on that side of the road, and ultimately link to a future connection to the greenway recommended along the Billy Branch stream. Pedestrian signals should be considered on each end of the bridge to facilitate continued travel along the west side of the road. A small buffer area could also be considered along the bridge's wide shoulder area, consisting of striping or fold over bollards.

Planning Level Cost Estimate: \$1,030,000





G4: Berkeley Boulevard Bicycle and Pedestrian Facilities #1

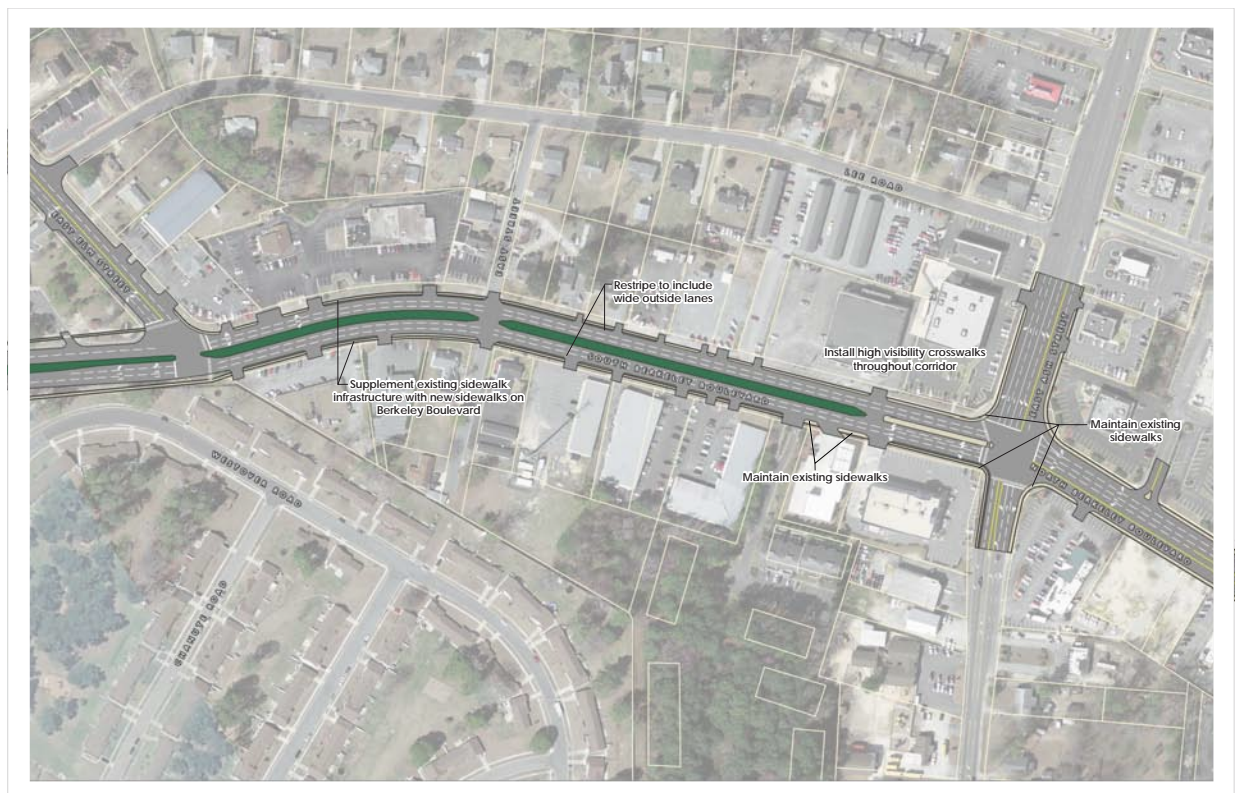
Berkeley Boulevard between Ash Street and Elm Street serves not only the large number of commercial developments along either side of the road, but also serves as the primary entry point to Seymour Johnson Air Force Base. This corridor currently has no provisions for bicycles, and only limited sidewalk connections. The recommended treatment is to construct sidewalks along both sides of Berkeley Boulevard, make pedestrian crossing improvements at the intersections with Ash Street and Elm Street, and restripe the roadway to include wide outside lanes.

Extents and Facility Type: Ash Street to Elm Street: Sidewalks on Both Sides; Ash Street to Elm Street: Wide Outside Lanes (Restripe)

Overview and Purpose

The sidewalk proposed along both sides of Berkeley Boulevard should be five to six feet wide, with a three foot verge wherever possible. In light of existing right of way constraints, there may be sections of the sidewalk that will need to be directly on the back of the curb. If possible, the sidewalk should be greater than five feet wide in these locations so pedestrians have a greater potential separation from motorized traffic. The sidewalk on the east side of Berkeley Boulevard should ultimately tie in to the existing sidewalk along the east side of Wright Brothers Avenue. The recommended sidewalk will also tie into the existing sidewalk currently in place along Berkeley Boulevard, creating a continuous pedestrian travelway. High visibility crosswalks with countdown pedestrian signals are recommended at the intersections with Elm Street and Ash Street. The exhibit shows these improvements and indicates the preferred approaches for pedestrian crossings at these intersections, as well as the location of a pedestrian refuge for users crossing Berkeley Boulevard at Ash Street. Wide outside lanes along Berkeley Boulevard will allow bicyclists comfortable with riding in the travel lanes a greater comfort measure while traveling on this road.

Planning Level Cost Estimate: \$410,000



G5: Berkeley Boulevard Bicycle and Pedestrian Facilities #2

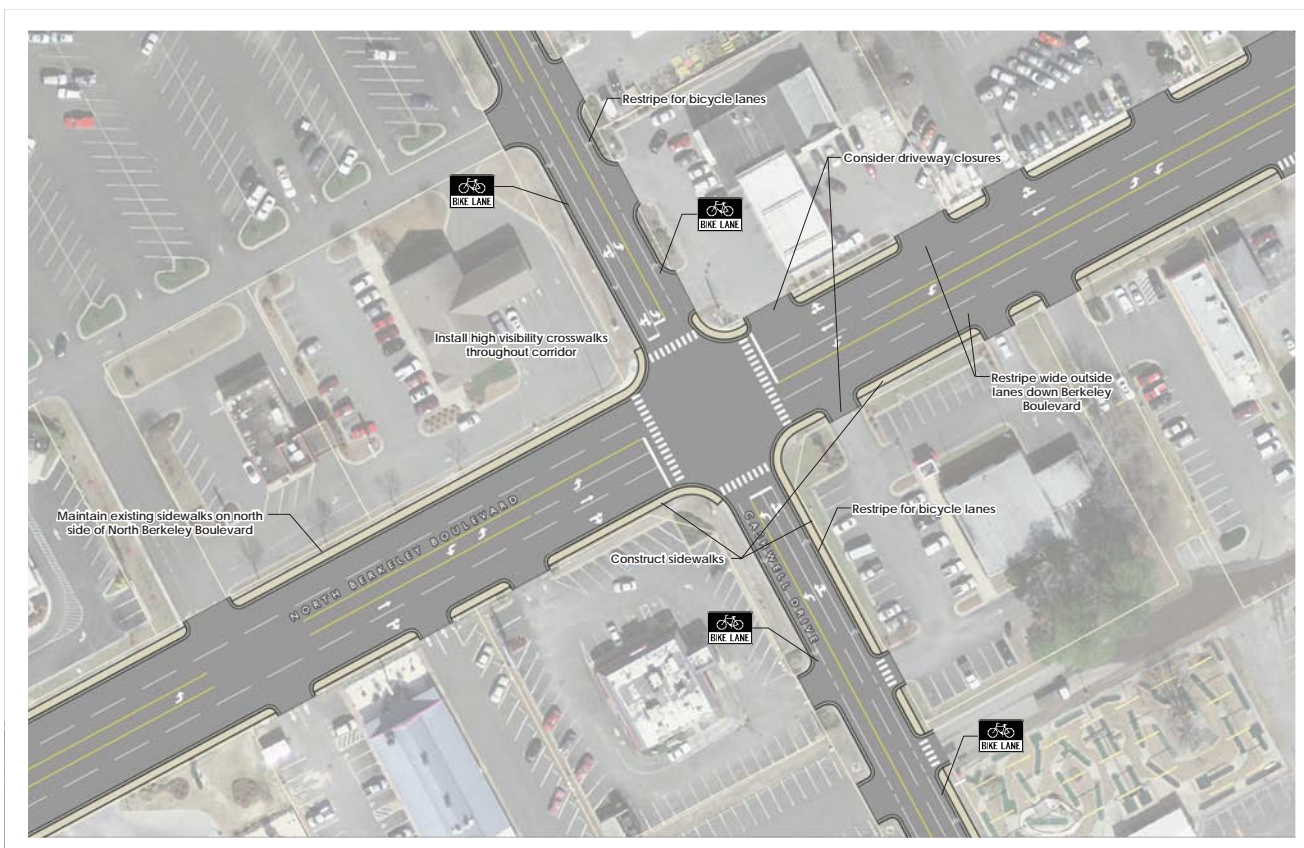
As with the section in project G4, this section of Berkeley Boulevard serves primarily commercial uses, notably including the Berkeley Mall. Intersecting roadways provide linkages to nearby single and multifamily residential areas. This area is currently served by a sidewalk on the north side of the street, and very limited sidewalks on the south side. The recommended treatment is to construct sidewalks on the south side of Berkeley Boulevard as well as restriping the roadway to accommodate wide outside lanes.

Extents and Facility Type: Ash Street to US 70 Bypass: Sidewalks on Both Sides; Ash Street to US 70 Bypass: Wide Outside Lanes (Restripe)

Overview and Purpose

Sidewalks along Berkeley Boulevard are recommended to be six feet wide. In order to interface with current sidewalk infrastructure and stay within the available right of way, proposed sidewalks will be located directly on back of curb. The proposed wide outside lanes continue the section recommended in project G4 and provide a more comfortable travel space for on-road bicyclists. The exhibit shows the proposed improvements to the Berkeley Boulevard and Cashwell Drive intersection. Improvements such as high visibility crosswalks at the intersection are shown, which would be complemented by pedestrian countdown signals. Planned bicycle lanes along Cashwell are shown as well. In order to further enhance safety near this intersection and increase driver predictability, there are also driveway closures noted on the westbound approach of Berkeley Boulevard. In both instances these closures affect a parcel with multiple access points along both Berkeley Boulevard and Cashwell Drive. Eliminating these driveway openings will create a more logical traffic pattern and reduce conflict points between vehicles and pedestrians.

Planning Level Cost Estimate: \$970,000





G6: Elm Street Bicycle and Pedestrian Improvements

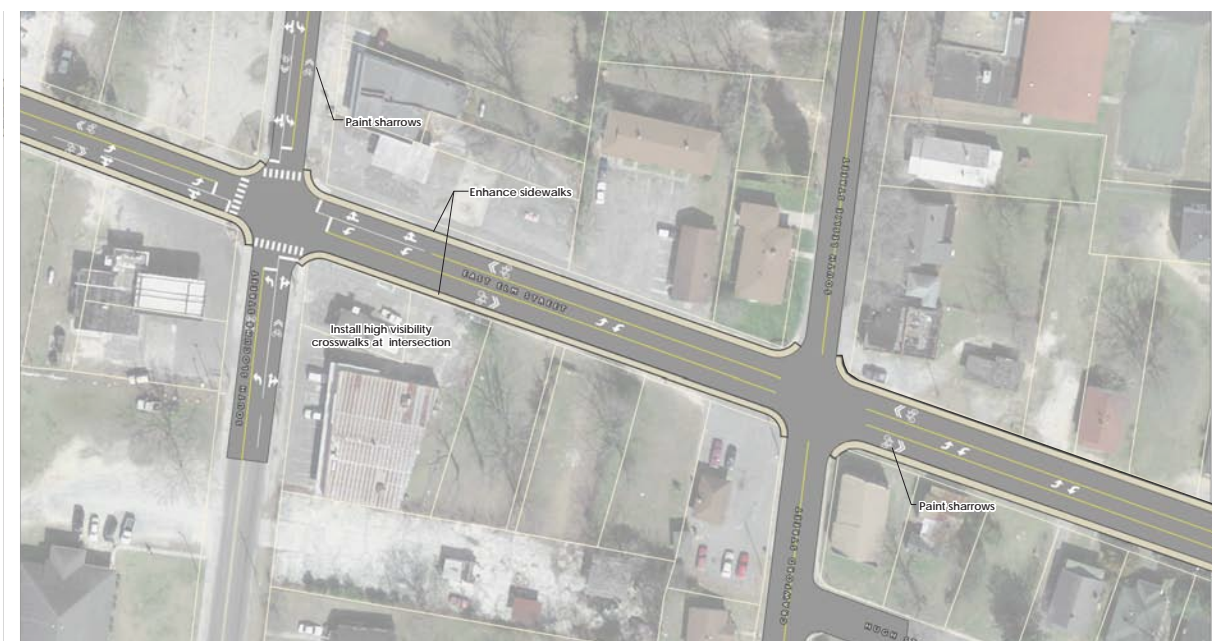
Elm Street is a major east-west corridor through the City of Goldsboro, providing a continuous link between US 13 and Seymour Johnson Air Force Base. However, between Slocumb Street and Berkeley Boulevard this roadway is predominantly residential in nature. There are currently no bicycle or pedestrian amenities serving this area. The recommended improvements would provide sidewalks on both sides of the roadway, and sharrows along all or part of the corridor.

Extents and Facility Type: Slocumb Street to Berkeley Boulevard: Sidewalks on Both Sides; Slocumb Street to Berkeley Boulevard: Paint Sharrows

Overview and Purpose: Elm Street serves as the major parallel route to Ash Street to the south. While each of these roadways have a variety of specific commercial and residential users, they also both have the potential to serve through traffic. As a result, the recommendations developed for Elm Street (project G6) and Ash Street (projects G9 and G10) were considered together to ensure overall regional mobility needs would still be accommodated. Each of these roadways were explored for the possibility of implementing a road diet. For Elm Street, this road diet would have included the section between Randolph Street and Berkeley Boulevard. For Ash Street, the road diet would include the section between Audubon Avenue and Berkeley Boulevard. After examining existing and 2040 projected travel volumes, each section was shown to be a viable candidate for a road diet. However, it was acknowledged that completing a road diet along both corridors might result in negative ramifications for east-west mobility within the City of Goldsboro, particularly as concerns access to Seymour Johnson Air Force Base. As a result, a road diet is recommended for Ash Street but not for Elm Street.

Recommendations for Elm Street include constructing sidewalks on both sides of the street. Sidewalks will likely remain on back of curb due to right of way constraints. Sharrows are recommended for this roadway to help signal the presence and potential locations of bicyclists. Elm Street was evaluated for inclusion of striped bicycle lanes. However, given the current roadway width of 32 feet, three additional feet of pavement would be needed on each side of the roadway to accommodate these lanes. With the right of way constraints along this corridor, this is likely not a feasible option. The exhibit shows the proposed sidewalk and sharrow improvements at the intersection of Elm Street and Slocumb Street, along with high visibility crosswalk improvements at that location.

Planning Level Cost Estimate: \$1,630,000



G7: Herman Street Sidewalks

The portion of Herman Street between Beech Street and Royall Avenue connects such bicycle and pedestrian friendly destinations as Herman Park, historic Goldsboro High School, and the Wayne Middle High School Academy. Sidewalks exist on a portion of this road near Goldsboro High School, but do not continue northward. The recommended improvements would continue the sidewalk along the east side of the road with pedestrian crossing improvements at major intersections.

Extents and Facility Type: Royall Avenue to Beech Street: Sidewalk on East Side

Overview and Purpose

Near its intersection with Beech Street, Herman Street is a three lane roadway. It widens to a five lane section north of Dortch Street, which results in a significantly more constrained right of way. Where possible, the recommended sidewalk should be separated by a wide verge area consistent with what exists near Goldsboro High School. The exhibit shows the intersection of Herman Street and Beech Street. At this signalized intersection, high visibility crosswalks should be installed along with pedestrian countdown signals. In order to facilitate continuity for the pedestrian, sidewalks should be extended along the east side of Herman Street south of the intersection.

Planning Level Cost Estimate: \$260,000





G8: Harris Street/Bunche Drive Bicycle and Pedestrian Facilities

The corridor of Bunche Drive and Harris Street serves primarily single family residential uses. The Carver Heights Elementary School and Dillard Middle School connect along this corridor off of Stadium Drive. A short section of sidewalk exists along the middle of Harris Street; however, no other bicycle or pedestrian facilities serve this corridor. The recommended improvements would add a sidewalk along one side of this corridor, along with sharrows or striped bicycle lanes where possible.

Extents and Facility Type: John Street to Stadium Road: Sidewalk on North Side; Stadium Road to Stoney Creek Parkway: Sidewalk on South Side; John Street to Slocumb Street: Bicycle Lane (Stripe); Slocumb Street to Porter Street: Sharrows (Stripe); Porter Street to Stoney Creek Parkway: Bicycle Lane (Stripe)

Overview and Purpose: As mentioned previously, this corridor serves single family residential uses, many of which front the roadway. Right of way constraints, utility locations, and the section of existing sidewalk were all considered when determining the placement of sidewalks in this area. The sidewalk on the north side of Bunche Drive between John Street and Stadium Drive directly serves the needs of the elementary school. Intersection improvements are recommended at Stadium Drive to assist pedestrians with crossing the street as the sidewalk shifts to the south side. Since this is an unsignalized intersection these improvements could include high visibility crosswalks along with amenities such as a rectangular rapid flashing beacon (RRFB), pedestrian hybrid beacon (PHB, formerly known as a HAWK signal), or even a crossing guard during school hours. The sidewalk continues along the south side of the roadway for the remainder of its length, thereby reducing conflicts caused by multiple crossings. The width of the roadway is sufficient within the sections of Bunche Drive between John Street and Slocumb Street as well as Harris Street between Porter Street and Stoney Creek Parkway to restripe for bicycle lanes. In the section between Slocumb Street and Porter Street, sharrows are recommended to continue to provide a higher level of visibility for bicyclists.

Planning Level Cost Estimate: \$1,420,000



G9: Ash Street Road Diet Vision Project

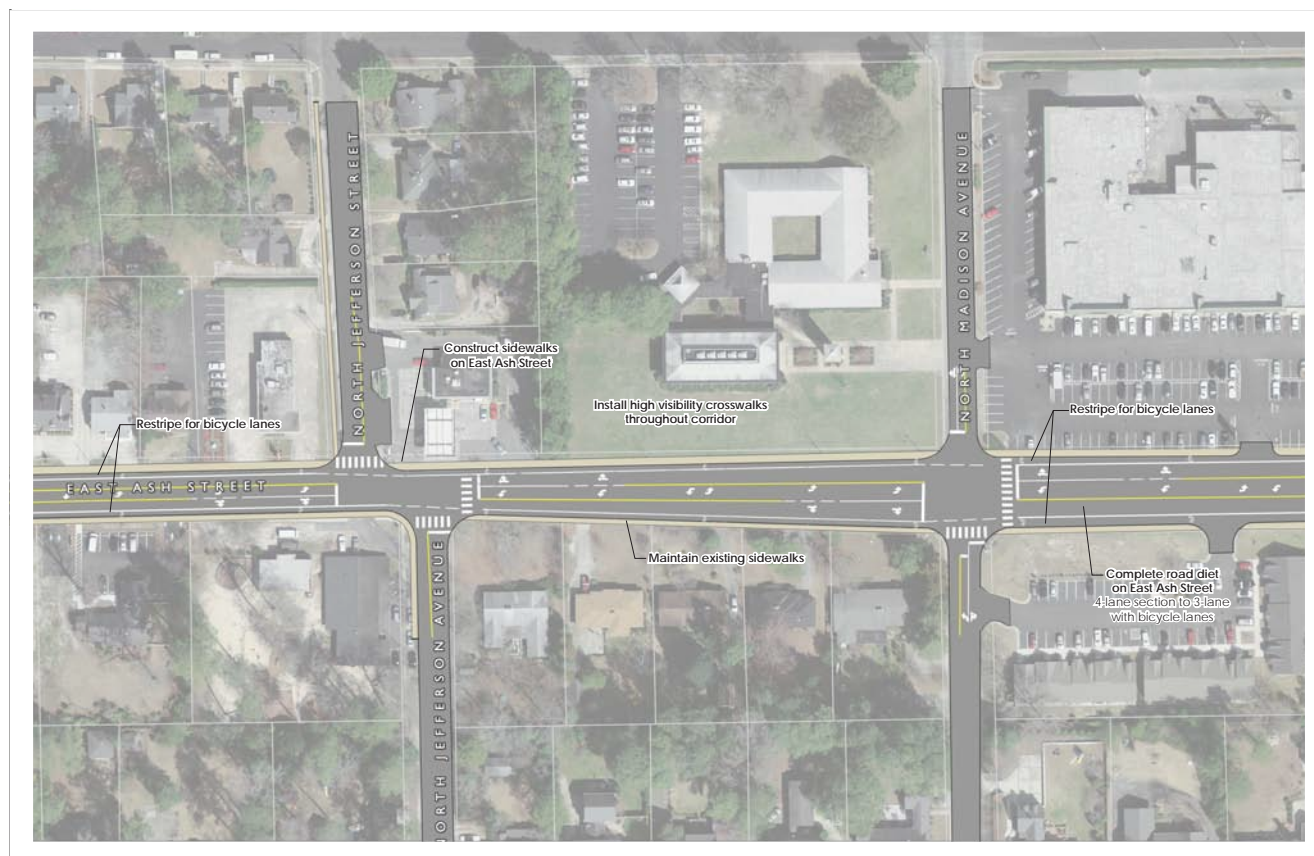
Ash Street, also designated as US 70 Business, is the main east-west corridor within the City of Goldsboro. Ash Street serves a variety of commercial and residential uses and connects to many more residential areas. Ash Street is also a major connection to Stoney Creek Park. There are currently no bicycle amenities along this corridor. Sidewalks run along the south side of the corridor, with only patchy sidewalks to the north. The recommended improvement would use a road diet to stripe bicycle lanes through the entire corridor.

Extents and Facility Type: Audubon Drive to Berkeley Boulevard: Bicycle Lanes (Road Diet)

Overview and Purpose

As discussed with project G6, both Elm Street and Ash Street were considered as candidates for road diets. Given the high number of potential bicycle and pedestrian trip generators and attractors along Ash Street, this street was identified as the preferred corridor. The road diet includes existing four and five lane sections, taking the entire corridor to three lanes with striped bicycle lanes. The exhibit shows a partial section of Ash Street between Jefferson Street and Madison Avenue. The original cross-section changes from a four to five lane section between these two roadways. The exhibit details how the existing pavement could be used as part of the three lane section. Sidewalk improvements and pedestrian enhancements described as project G10 are also shown in this exhibit.

Planning Level Cost Estimate: \$530,000





G10: Ash Street Sidewalk

Ash Street, also designated as US 70 Business, is the main east-west corridor within the City of Goldsboro. Ash Street serves a variety of commercial and residential uses and connects to many more residential areas. Ash Street is also a major connection to Stoney Creek Park. There are currently no bicycle amenities along this corridor. Sidewalks run along the south side of the corridor, with only patchy sidewalks to the north. The recommended improvement would construct a sidewalk along the north side of the roadway along with intersection crossing improvements at major intersections.

Extents and Facility Type: Audubon Avenue to Berkeley Boulevard: Sidewalk on North Side

Overview and Purpose

In order to create a more accessible pedestrian travelway, this project will result in sidewalks along both sides of Ash Street. Due to right of way restrictions the sidewalk on the north side of Ash Street will likely be located directly on the back of curb. With this in mind, the sidewalk is recommended to be six feet wide, with the added width providing a greater comfort level for pedestrians. In addition, high visibility crosswalks are recommended at major intersections, as detailed in the exhibit. Signalized intersections should also include pedestrian countdown signals. The intersection of Claiborne Street is unsignalized. As a result, this intersection will need a yield to pedestrians in crosswalk sign.

Planning Level Cost Estimate: \$890,000



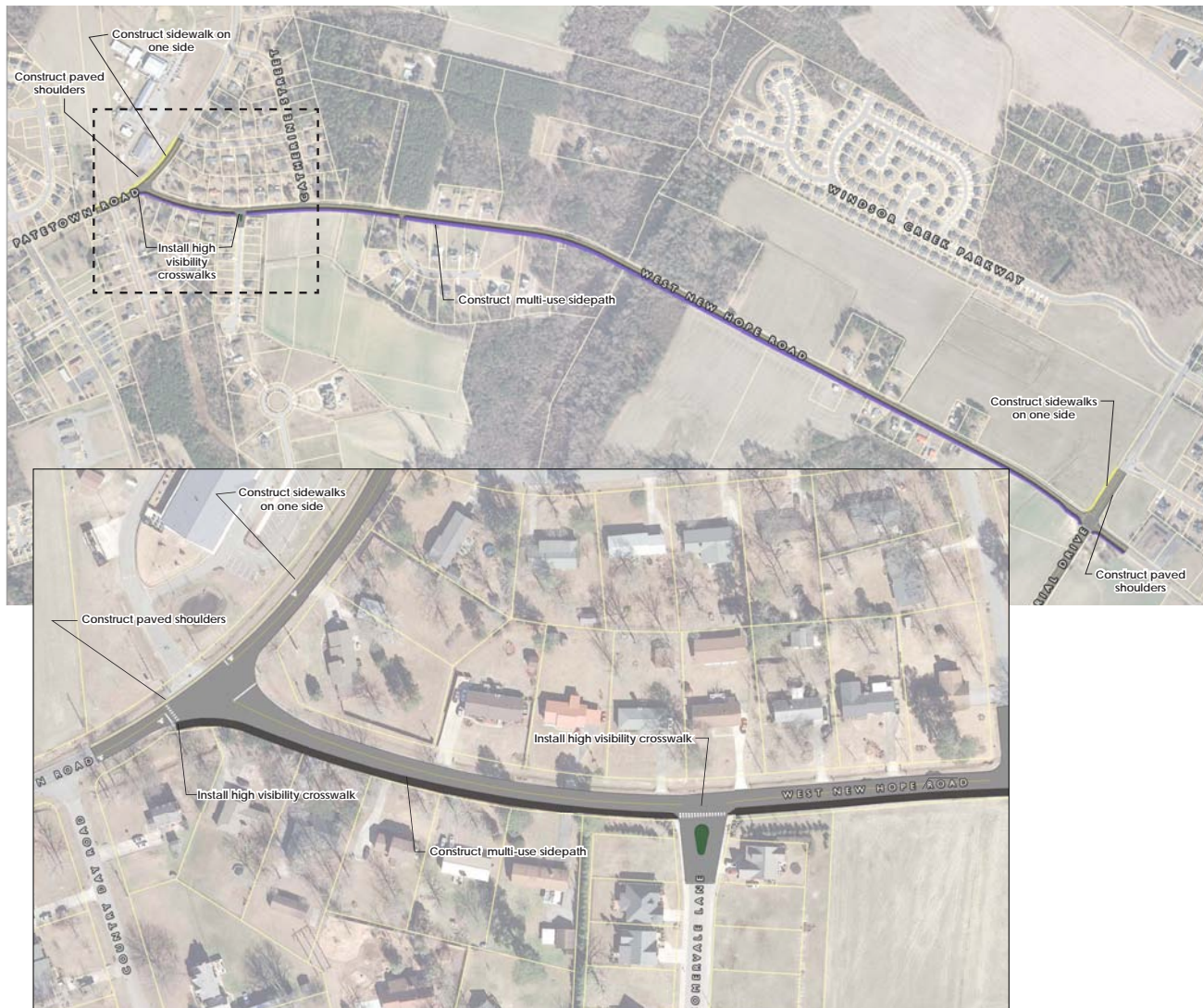
G11: New Hope Road Sidepath

New Hope Road is a major connecting facility on the north side of the City of Goldsboro. The road serves primarily rural and residential uses. The recommended improvement includes a sidepath along the south side of New Hope Road along with crossing improvements along the way.

Extents and Facility Type: Wayne Memorial Drive to Patetown Road: Sidepath on South Side

Overview and Purpose: The recommended sidepath will ultimately connect with existing improvements to the east on New Hope Road. The suitability of this roadway was examined to determine the most appropriate location for the sidepath. Based on this assessment, the south side of New Hope Road was determined as the best location for the sidepath. Right of way along the corridor is sufficient to allow for a verge between the sidepath and the roadway. However, at the far western end of the corridor near the Patetown Road intersection, this verge area will be smaller. Curb and gutter will be needed along the south side of the road to accommodate the sidepath and verge within currently available right of way. This project is depicted in two exhibits. The first exhibit shows the overall placement of the sidepath and identifies intersections where crossing improvements will be needed. The second exhibit shows the intersections of Patetown Road and Somervale Lane with New Hope Road, along with the crossing improvements needed to accommodate the sidepath and interface with a proposed sidewalk along Patetown Road.

Planning Level Cost Estimate: \$970,000





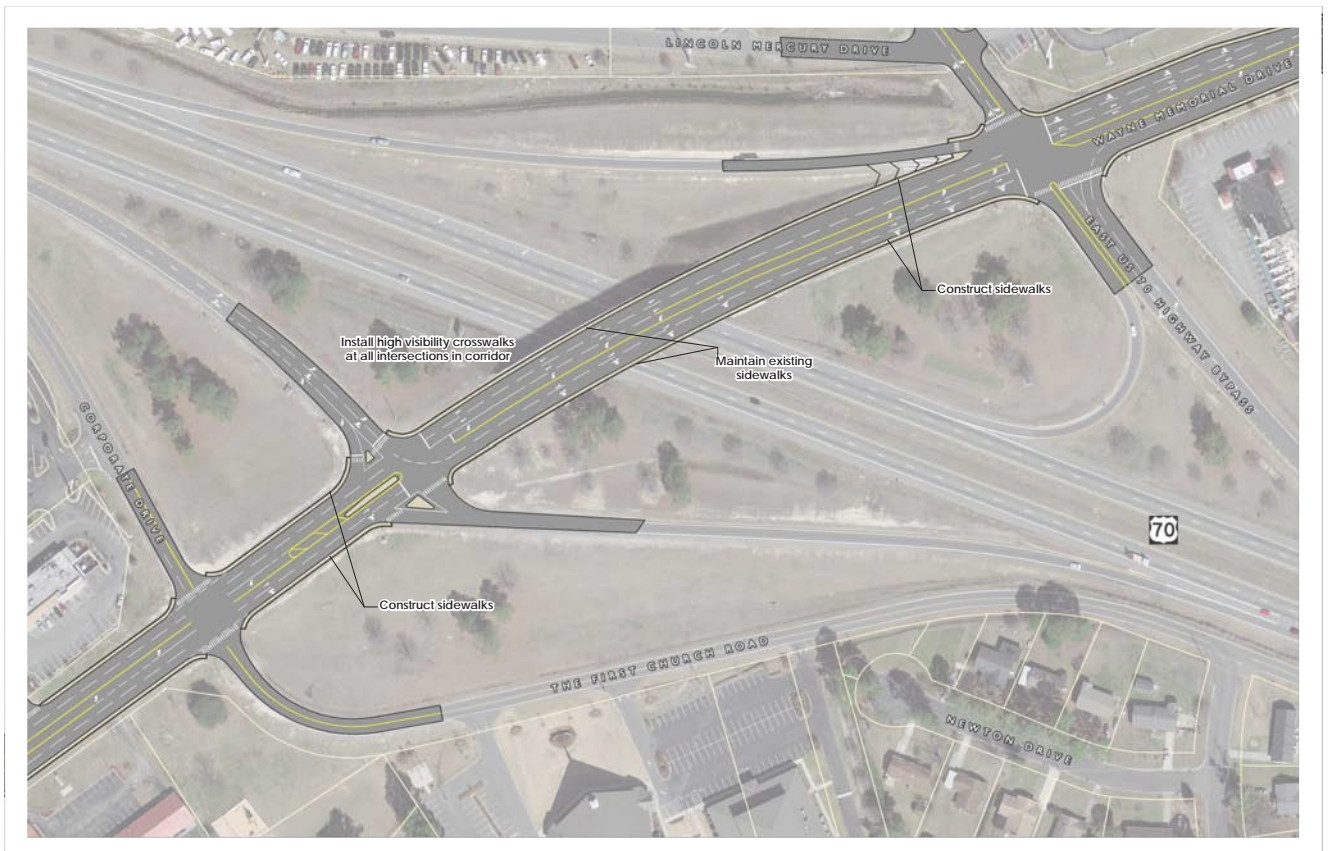
G12: Wayne Memorial Drive Sidewalks

Wayne Memorial Drive near the US 70 Bypass interchange is a major commercial corridor. Sidewalks currently exist on both sides of the US 70 Bypass overpass, and on the south side of Wayne Memorial Drive to the east of the interchange. The recommended improvement would construct sidewalks along both sides of this roadway as well as intersection crossing improvements.

Extents and Facility Type: Royall Avenue to Country Day Road: Sidewalks on Both Sides

Overview and Purpose: Wayne Memorial Drive is a heavily traveled and wide roadway. Sidewalks along both sides of the road will provide pedestrians a safer place to travel without unnecessary crossing. However, if sufficient funding is not available to construct sidewalks on both sides at once, this project could be phased. Considering the relative ease of crossing improvements needed as well as the presence of existing sidewalks on the south side with which connections could be made, the south side sidewalks would likely be the best candidates for an interim phase. Intersection crossing improvements along the corridor include high visibility crosswalks and pedestrian countdown signals. At the East US 70 Bypass ramp, a two part crossing will be needed along the north side. Sidewalk connections in that area will likely require fencing or guard rail to separate pedestrians from drop-offs in adjacent terrain.

Planning Level Cost Estimate: \$1,000,000





G13: New Hope Road and Harding Road Intersection Improvements

New Hope Road has an existing sidepath that terminates as a wide shoulder at the intersection with Harding Drive. South Harding Drive currently provides striped bicycle lanes. Some limited signage exists to direct users between the facilities, but crossing locations are unclear. The recommended improvements consider an interim enhancement to striping and signage, as well as a long term improvement to realign the intersection.

Extents and Facility Type: New Hope Road at Harding Road: Intersection Improvements

Overview and Purpose

At this time, South Harding Drive and North Harding Drive approach New Hope Road at an offset. This offset leads to confusion and adds conflict points. As a result, realigning the south leg of the intersection to line up with North Harding Drive is the preferred solution in this area. A cursory review of this improvement indicates that minimal right of way will need to be acquired; however, significant cost would be incurred in realigning the intersection. The first exhibit shows the recommended long term intersection realignment. High visibility crosswalks are recommended at all intersection legs. Additional width is recommended to be added to the wide shoulder portion of the sidepath to meet minimum NCDOT width requirements. Added width can also accommodate striped buffer or fold over bollards, thereby providing some separation from motorists. A bicycle route sign should be placed at the intersection guiding bicyclists from the bicycle lanes on Harding Drive onto the New Hope Road sidepath.

The second exhibit shows the recommended interim improvements. These improvements are based around creating a safer and more predictable crossing experience that makes use of the existing pavement. To do this, the section of the sidepath that is a wide shoulder section is recommended to be painted. This paint will help call attention to the unique function of the shoulder in this area. The painting is recommended to be carried through the high visibility crosswalk area. A bicycle route sign should be placed at the intersection guiding users from the bicycle lanes on Harding Drive onto the New Hope Road sidepath.

Planning Level Cost Estimate:

Interim Improvements: \$16,000

Long-Range Improvements: Additional Study Needed

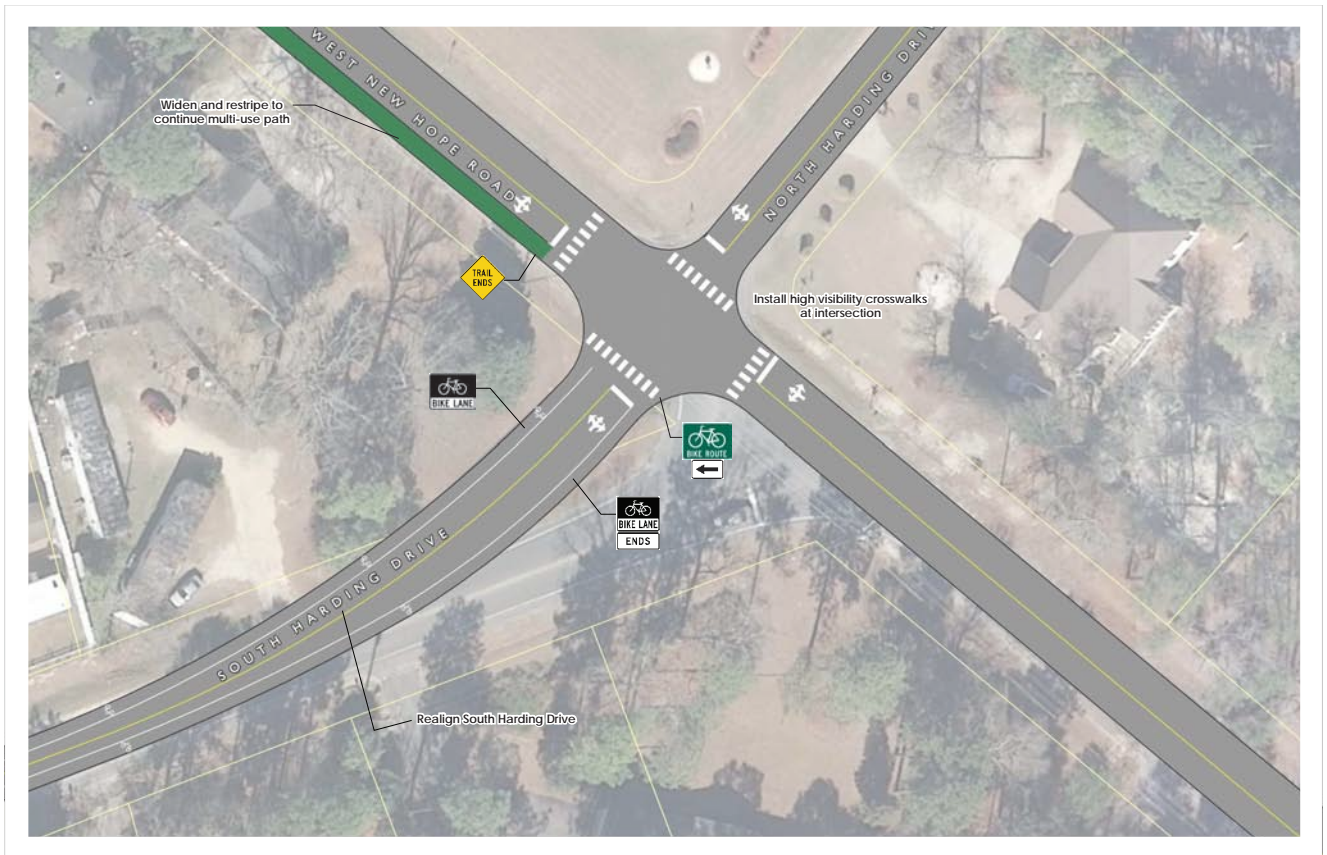


Exhibit One: Long-Term Improvement

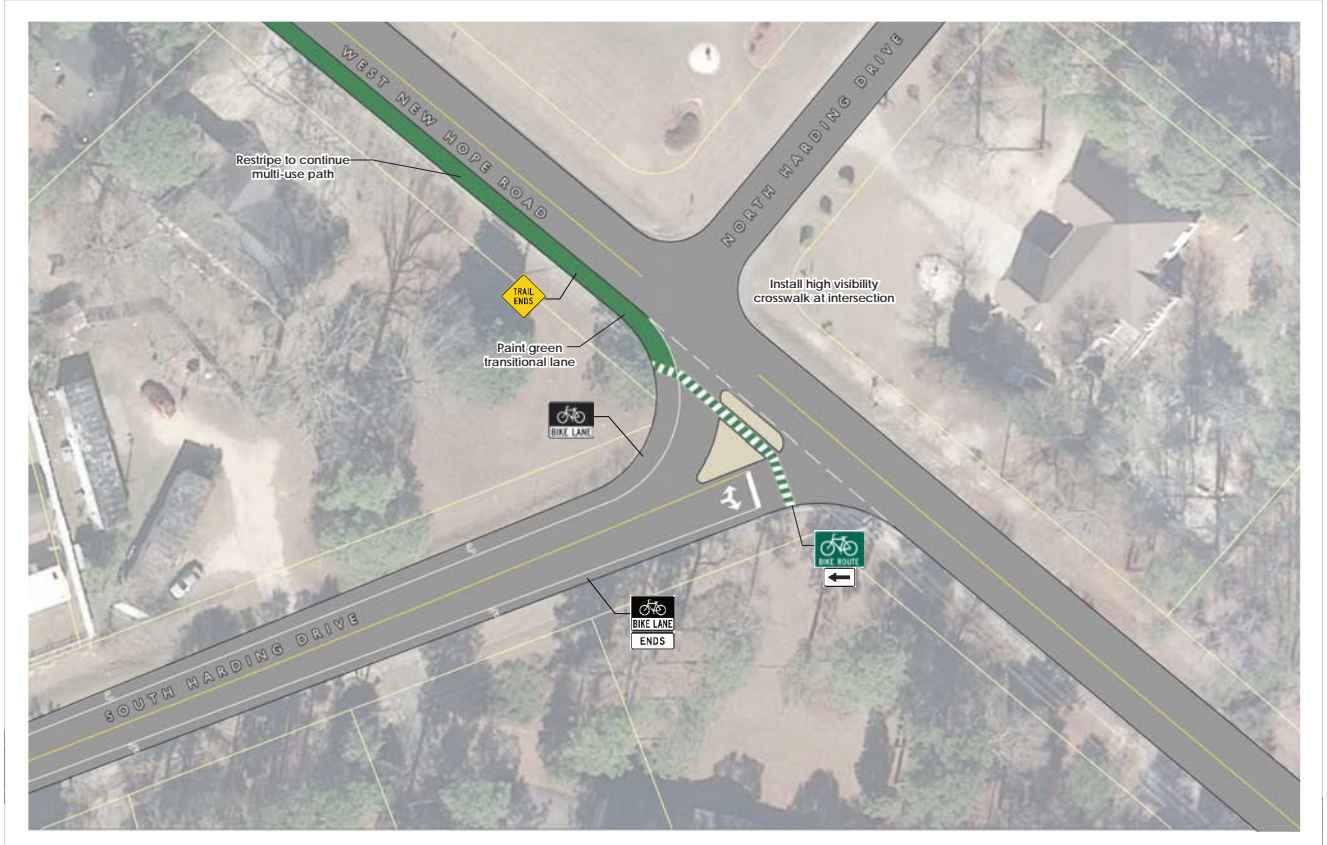
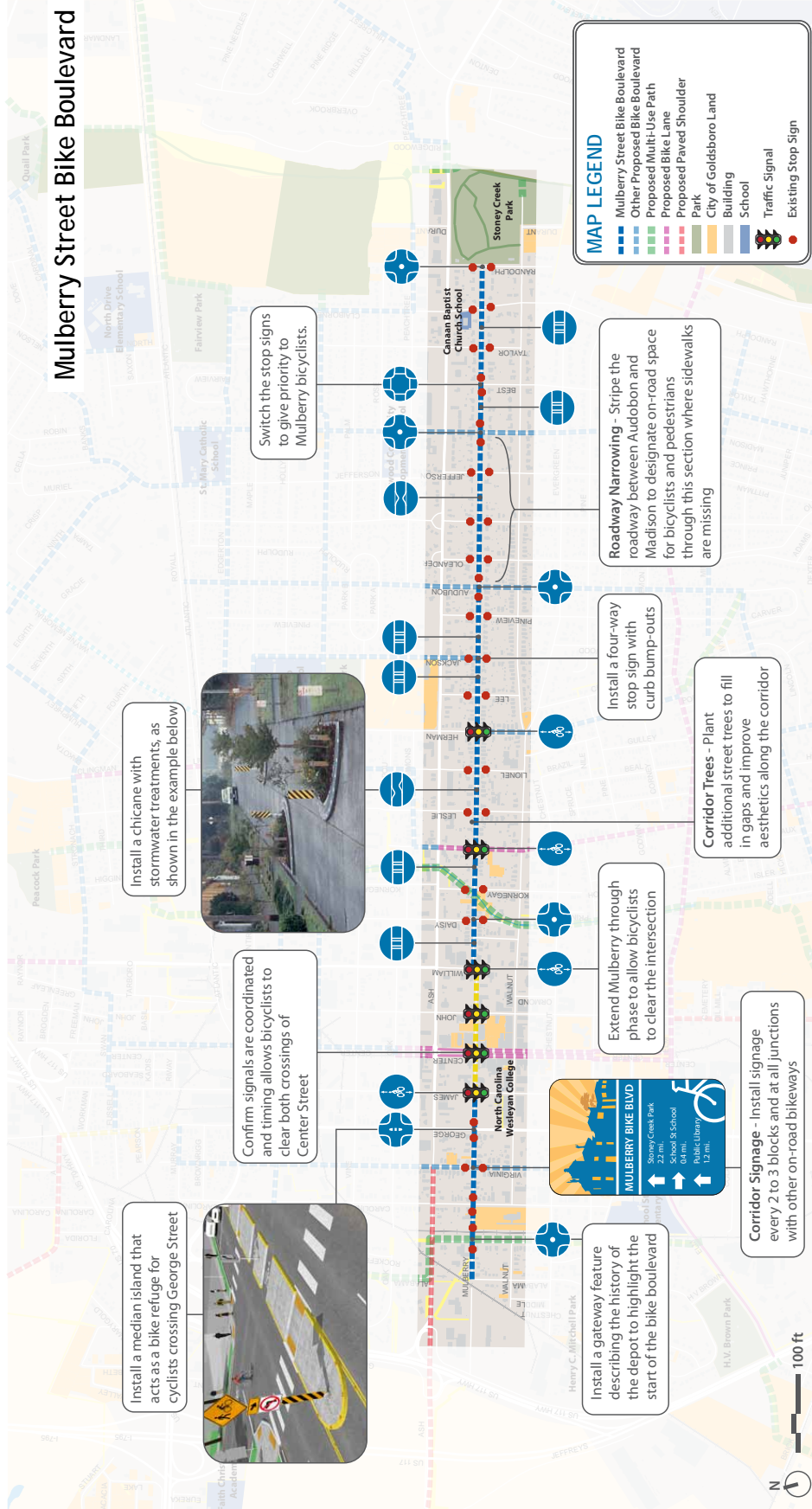


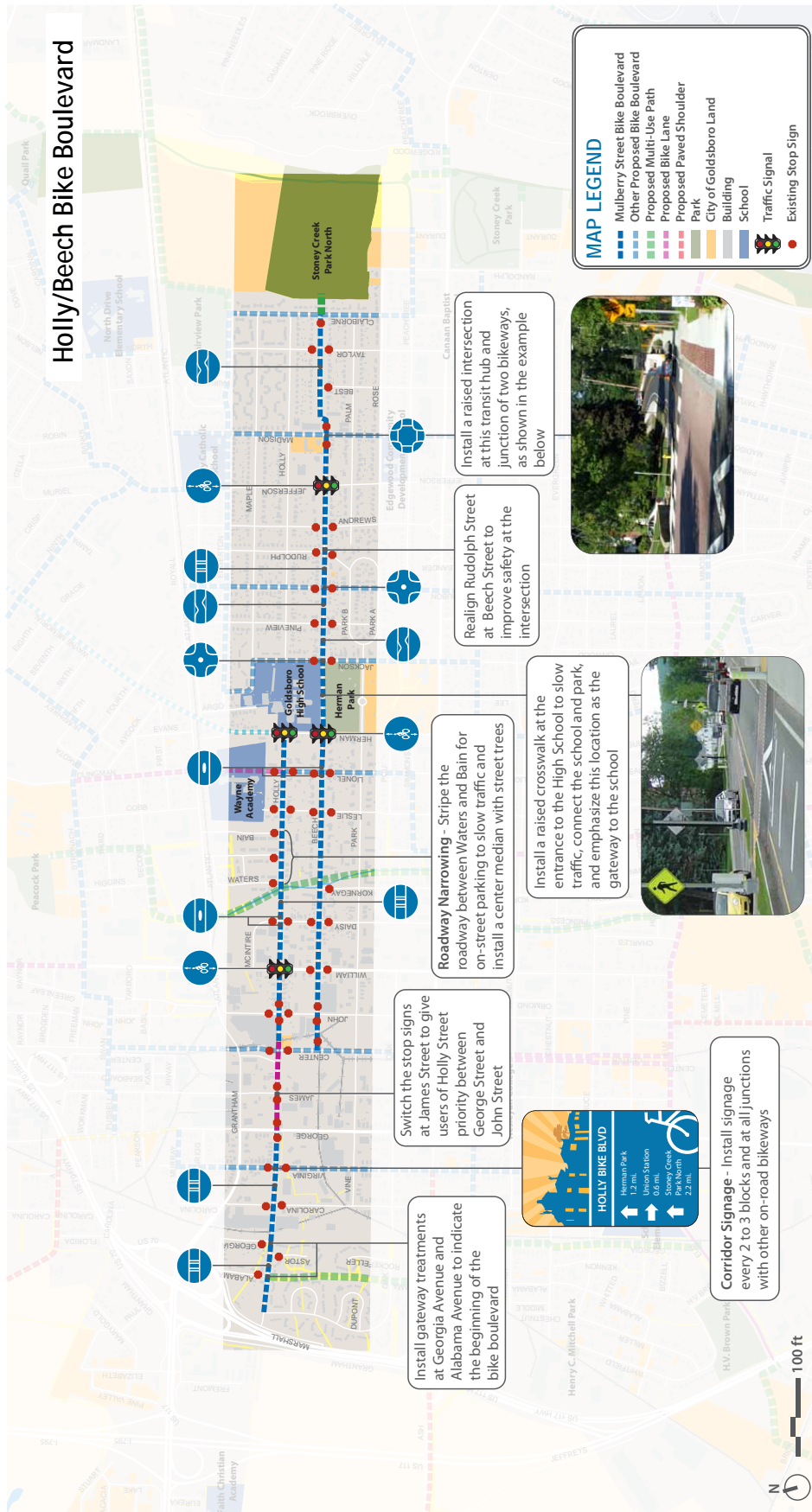
Exhibit Two: Interim Improvement

CITY OF GOLDSBORO BIKE BOULEVARD PROJECTS

The following are conceptual recommendations for selected bike boulevard projects identified in this Plan. Cost estimates were not determined.



- BICYCLE BOULEVARD TREATMENT TOOLBOX**
 See Design Guidelines Appendix for detailed guidance on this toolkit
- Horizontal Deflection**
 - Traffic Circle** - Raised or delineated islands placed at intersections
 - Chicane** - A series of raised curb extensions, or edge islands on alternate sides of a street
 - Vertical Deflection**
 - Speed Humps** - Raised area 12 to 14" long by 3 to 4 inches high that reduce speeds to 15 to 20 mph
 - Raised Intersection** - Raised area including crosswalk space and the full area of the intersection
 - Major Street Crossings**
 - Bike Detection** - Bicycle-activated detectors that allow the presence of a bicyclist to trigger a traffic signal
 - Median Island** - An island refuge for cyclists crossing multi-lane roadways that restricts vehicle turning movements
 - Choker** - Edge islands placed on either side of the street to narrow the center of the lane
 - Median** - Center island parallel to the bicycle boulevard that causes deflection



Holly/Beech Bike Boulevard

MAP LEGEND

- Mulberry Street Bike Boulevard
- Other Proposed Bike Boulevard
- Proposed Multi-Use Path
- Proposed Bike Lane
- Proposed Paved Shoulder
- Park
- City of Goldsboro Land
- Building
- School
- Traffic Signal
- Existing Stop Sign

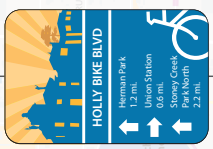
Install gateway treatments at Alabama Avenue and Georgia Avenue to indicate the beginning of the bike boulevard

Switch the stop signs at James Street to give users of Holly Street priority between George Street and John Street

Roadway Narrowing - Stripe the roadway between Waters and Bain for on-street parking to slow traffic and install a center median with street trees

Realign Rudolph Street at Beech Street to improve safety at the intersection

Install a raised intersection at this transit hub and junction of two bikeways, as shown in the example below



Corridor Signage - Install signage every 2 to 3 blocks and at all junctions with other on-road bikeways



BICYCLE BOULEVARD TREATMENT TOOLBOX

See Design Guidelines Appendix for detailed guidance on this toolkit

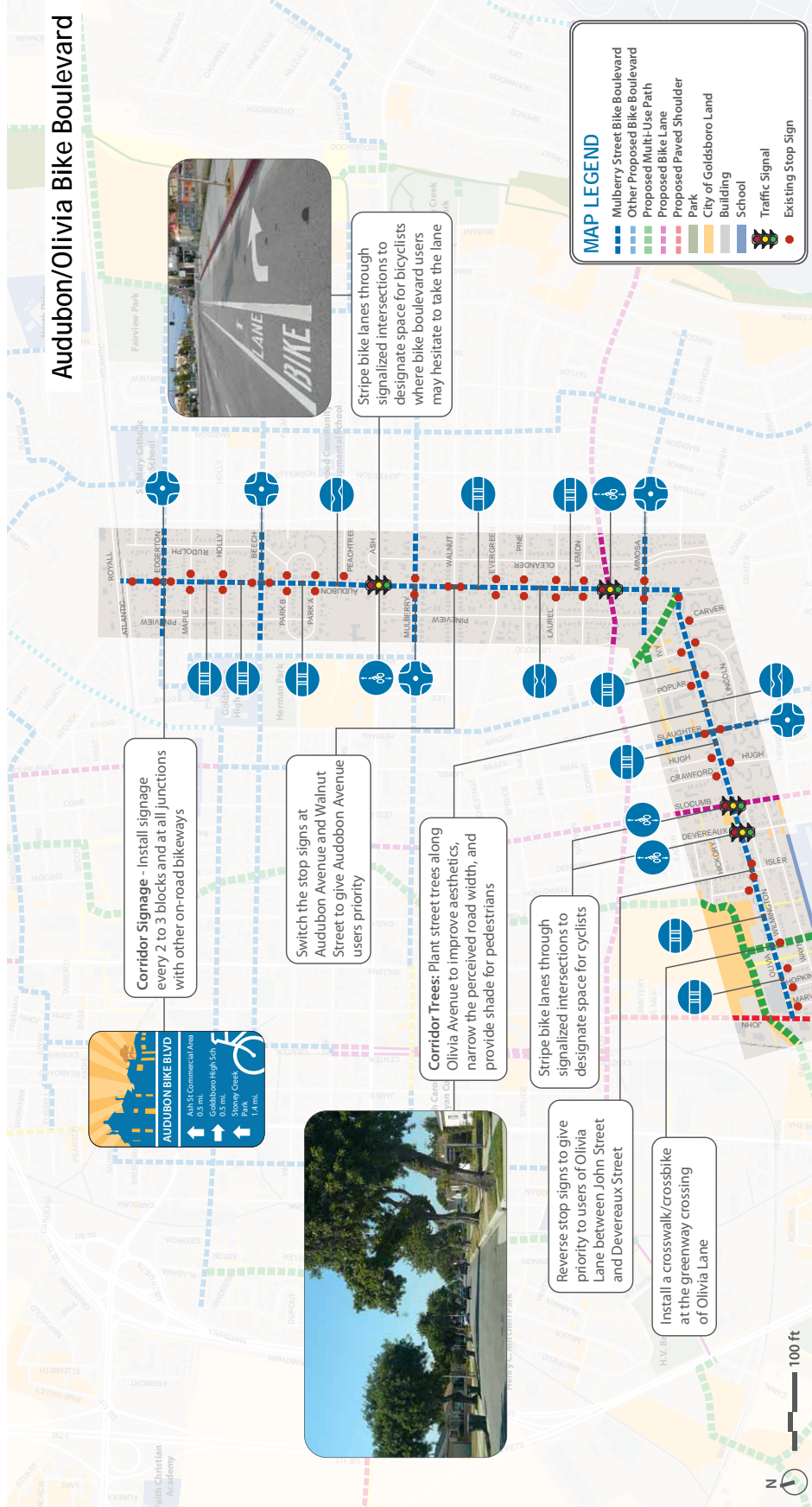
- Horizontal Deflection**
- Traffic Circle** - Raised or delineated islands placed at intersections
 - Chicane** - A series of raised curb extensions, or edge islands on alternate sides of a street

- Vertical Deflection**
- Choker** - Edge islands placed on either side of the street to narrow the center of the lane
 - Median** - Center island parallel to the bicycle boulevard that causes deflection

- Major Street Crossings**
- Bike Detection** - Bicycle-actived detectors that allow the presence of a bicyclist to trigger a traffic signal
 - Median Island** - An island refuge for cyclists crossing multi-lane roadways that restricts vehicle turning movements

- Vertical Deflection**
- Speed Humps** - Raised area 12 to 14' long by 3 to 4 inches high that reduce speeds to 15 to 20 mph
 - Raised Intersection** - Raised area including crosswalk space and the full area of the intersection

Major Street Crossings



BICYCLE BOULEVARD TREATMENT TOOLBOX

See Design Guidelines Appendix for detailed guidance on this toolkit



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WALNUT CREEK PROJECTS

WC1: Walnut Creek Drive Sidewalks

Walnut Creek Drive serves as the main gateway to the Village of Walnut Creek from US 70. This roadway is a two lane facility surrounded by residential uses with a speed limit of 30 miles per hour. There are currently no bicycle or pedestrian facilities along this roadway. The recommended treatment is to construct sidewalks on both sides of the road.

Extents and Facility Type: US 70 to Mill Road: Sidewalks on Both Sides

Overview and Purpose

The addition of sidewalks on both sides of this roadway will facilitate safer pedestrian travel within the Village of Walnut Creek. Five foot sidewalks are recommended, along with a two to three foot verge area. The restricted right of way of this roadway limits the ability to include a larger verge area. In addition, the current shoulder section would likely need to be converted to curb and gutter to assist in fitting within current right of way. Given the low speed of this roadway, sidewalk could be placed on back of curb where constraints prohibit the inclusion of a verge. High visibility crosswalks should be included at the intersections of Tramway Drive and Mill Road. Mini traffic circles may also be considered in these locations for aesthetic purposes as well as to reinforce the desired travel speed. Due to the low speed of this roadway, bicycles should be able to ride in the travel lanes without special accommodations.

Planning Level Cost Estimate: \$430,000





WC2: Mill Road Sidewalks

Mill Road is a significant connecting road within the Village of Walnut Creek. This road is two lanes with a speed limit of 30 miles per hour. However, it is the only road within the Village limits that connects the northern and southern portions of the Village across Lake Wackena. No bicycle or pedestrian facilities currently exist on this road. The recommended treatment is to construct sidewalks on both sides of the road.

Extents and Facility Type: Walnut Creek Drive to Lake Wackena: Sidewalks on Both Sides

Overview and Purpose: Five foot sidewalks, along with a two to three foot verge area, are recommended along both sides of Mill Road. In order to accommodate right of way constraints, the existing ditch section would likely need to be converted to curb and gutter. Sidewalk additions along one side of the road could be considered as an alternative treatment in this area. Mini traffic circles may also be considered at the intersections of Mill Road with Walnut Creek Drive and Dogwood Trail for aesthetic purposes as well as to reinforce the desired travel speed. Due to the low speed of this roadway, bicycles should be able to ride in the travel lanes without special accommodations.

Planning Level Cost Estimate: \$240,000





PIKEVILLE PROJECTS

P1: Main Street Bicycle and Pedestrian Facilities

The Main Street corridor begins as Pikeville-Princeton Road near I-795, a rural two lane roadway. Beginning at Hooks Grove Church Road and heading east, this road assumes a suburban and then small urban character, ultimately coming into the center of Pikeville near Railroad Street. A center turn lane between Russell Drive and Fort Street gives way to a wide two lane section that is signed 20 miles per hour. The recommended treatment is to construct sidewalks along both sides of this roadway, with sharrows in the three lane section transitioning to striped bicycle lanes in the downtown center.

Extents and Facility Type: I-795 to Goldsboro Street: Sidewalks on Both Sides; I-795 to Fort Street: Sharrows; Fort Street to Goldsboro Street: Bicycle Lane (Restripe)

Overview and Purpose

The section of Main Street between Russell Drive and Mill Street is primarily a residential one. However, the proximity of commercial uses between Mill Street and Goldsboro Street create an opportunity for convenient bicycle and pedestrian travel. The sidewalks recommended along this corridor promote pedestrian travel by getting potential users out of the street and onto dedicated facilities. High visibility crosswalks are recommended at all roadway intersections to help promote continuity of the pedestrian facilities. The signalized intersection of Goldsboro Street and Main Street also has recommended pedestrian signals with countdown clocks along all approaches. Curb and gutter already exists along Main Street, so a five foot sidewalk should be placed (where possible) at least two to three feet from the back of curb. Sidewalk on the north side only could be considered as an alternative if utility constraints prove too difficult to overcome. The bicycle recommendations make use of the existing roadway cross-sections. The sharrow between I-795 and Fort Street will help alert motorists that they should expect bicycles in this area. Striped bicycle lanes between Fort Street and Goldsboro Street will help visually reinforce the 20 mile per hour speed limit and provide a dedicated space for bicyclists.

Planning Level Cost Estimate: \$640,000





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Appendix J: Prioritization Tables

Appendix Contents:

Overview

Sidewalk Prioritization

*On-Road Bicycle
Prioritization*

*Shared-Use Path
Prioritization*

OVERVIEW

In order to determine the highest priority improvements recommended in this plan, a series of criteria were developed by which to rank each project. Sidewalk, on-road bicycle, and shared-use path recommendations were evaluated individually based on whether the project met the following criteria:

- » Low-income area (Based on Equity Analysis - see Chapter 2)
- » Low vehicle access area (Based on Equity Analysis - see Chapter 2)
- » High density population area
- » High minority population area (Based on Equity Analysis - see Chapter 2)
- » Reported pedestrian or bicycle crash location
- » Connectivity to or from an existing facility
- » Connectivity to or from proposed facilities
- » Top 5 recommendations from the public comment form
- » Park, library, or recreation center is within 1/2 mile radius
- » Elementary, middle, or high school is within 1/2 mile radius
- » Connectivity to major shopping center or business area
- » Connectivity to downtown
- » Community college is within 1/2 mile radius
- » Health focus area from the Goldsboro Comprehensive Plan
- » Connectivity to the Mountains-to-Sea (MST) alignment

Steering committee members ranked each criterion on a scale of 1-5 (1 = "Not Important, 5 = "Very Important"), and the scores from each committee member were aggregated and averaged to develop prioritization weights for each of the above criteria. These scores were then applied to each segment of recommended sidewalk, on-road bicycle facility, and shared-use path to rank projects, with the highest scores signifying the highest priorities for Goldsboro.

The following tables present the results for sidewalk, on-road bicycle, and shared-use path prioritization, ranked from highest to lowest. Projects highlighted in yellow have previously been identified as priorities by the City, separate from this process. They are identified here for reference.



SIDEWALK PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Side	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Wayne Memorial Dr	US 70	Lockhaven Dr	North	1,864	4.00	4.00	3.80	3.80	4.30
Ash St	Taylor St	Stoney Creek	North	2,080	4.00	4.00	0.00	3.80	4.30
Wayne Memorial Dr	Royall Ave	US 70	West/ North	3,829	4.00	4.00	0.00	3.80	4.30
Elm St	Slocumb St	Berkeley Blvd	North	10,497	4.00	4.00	3.80	3.80	4.30
Elm St	Slocumb St	Berkeley Blvd	South	10,643	4.00	4.00	3.80	3.80	4.30
Ash St	Jefferson Ave	Best St	North	1,145	4.00	4.00	0.00	3.80	4.30
Ash St	Audubon Ave	Jefferson Ave	North	1,343	4.00	4.00	0.00	3.80	4.30
Daisy St	Mulberry St	Magnolia St	East	811	4.00	4.00	3.80	3.80	0.00
Wayne Memorial Dr	Lockhaven Dr	Country Day Rd	North	1,205	4.00	4.00	3.80	3.80	4.30
Royall Ave	William St	Spence St	North	12,532	4.00	4.00	0.00	3.80	4.30
Slocumb St	Newsome St	Olivia Ln	East	650	4.00	4.00	3.80	3.80	0.00
George St	Elm St	Spruce St	East	750	4.00	4.00	0.00	3.80	0.00
Herman St	Beech St	Royall Ave	West	1,679	4.00	4.00	3.80	3.80	4.30
Spruce St	Railroad	George St	South	1,230	4.00	4.00	3.80	3.80	0.00
Spruce St	Railroad	James St	North	1,815	4.00	4.00	3.80	3.80	0.00
Slocumb St	Seymour Dr	Wisteria Rd	North	239	4.00	4.00	3.80	3.80	0.00
John St	Dixie Trail	Elm St	East	7,485	4.00	4.00	3.80	3.80	4.30
George St	Pine St	Spruce St	West	439	4.00	4.00	0.00	3.80	0.00
Carolina Ave	Pine St	Walnut St	East	1,022	4.00	4.00	0.00	3.80	0.00
William St	Atlantic Ave	US 70	East	4,038	4.00	4.00	0.00	3.80	4.30
Walnut St	Jefferson Ave	Stoney Creek Park	North	3,496	4.00	4.00	0.00	3.80	0.00
Slaughter St	Newsome St	Sidewalk	West	212	4.00	4.00	3.80	3.80	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Fills an Existing Sidewalk Gap</i>	<i>Community College within 1/2 Mile</i>	<i>Footpath Present</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	4.80	4.40	4.20	0.00	4.10	4.00	0.00	4.30	0.00	4.40	4.44	0.00	54.54
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	4.30	0.00	4.40	0.00	3.88	54.48
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	0.00	0.00	4.40	4.44	0.00	50.74
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	4.40	4.44	3.88	50.22
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	4.40	4.44	3.88	50.22
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	4.30	0.00	0.00	0.00	3.88	50.08
	4.80	0.00	4.20	4.30	4.10	4.00	0.00	4.30	0.00	4.40	0.00	3.88	50.08
	4.80	0.00	0.00	4.30	4.10	4.00	4.60	4.30	0.00	0.00	4.44	3.88	50.02
	4.80	0.00	4.20	0.00	4.10	4.00	0.00	4.30	4.10	0.00	4.44	0.00	49.84
	4.80	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	4.40	4.44	0.00	46.54
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	4.40	4.44	0.00	46.34
	4.80	0.00	4.20	4.30	4.10	0.00	0.00	4.30	0.00	4.40	4.44	3.88	46.22
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	4.40	0.00	0.00	46.20
	4.80	4.40	4.20	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	46.14
	4.80	4.40	4.20	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	46.14
	4.80	4.40	0.00	4.30	0.00	0.00	0.00	4.30	0.00	4.40	4.44	3.88	46.12
	4.80	4.40	0.00	4.30	4.10	0.00	4.60	0.00	0.00	0.00	4.44	0.00	45.94
	4.80	4.40	4.20	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	42.34
	4.80	4.40	4.20	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	42.34
	4.80	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	4.40	0.00	0.00	42.10
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	4.40	0.00	3.88	41.98
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94



SIDEWALK PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Side	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Newsome St	Slocumb St	Sidewalk	North	216	N	4.00	4.00	3.80	3.80
Newsome St	Sidewalk	Slaughter St	North	282	N	4.00	4.00	3.80	3.80
Olivia Ln	Deveraux St	Slocumb St	South	318	N	4.00	4.00	3.80	3.80
Jefferson Ave	Edgerton St	Royall Ave	East	769	N	4.00	4.00	3.80	3.80
Jefferson Ave	Edgerton St	Royall Ave	West	771	N	4.00	4.00	3.80	3.80
Edgerton St	Jefferson Ave	Madison Ave	North	817	N	4.00	4.00	3.80	3.80
Edgerton St	Jefferson Ave	Madison Ave	South	817	N	4.00	4.00	3.80	3.80
Wayne Memorial Dr	US 70	US 70	South	483	Y	4.00	4.00	0.00	3.80
Virginia Ave	Pine St	Sidewalk	West	518	N	4.00	4.00	3.80	3.80
Virginia Ave	Pine St	Sidewalk	East	967	N	4.00	4.00	3.80	3.80
Kornegay St	Ash St	Sidewalk	East	200	N	4.00	4.00	0.00	3.80
Kornegay St	Ash St	Magnolia St	West	334	N	4.00	4.00	0.00	3.80
Daisy St	Sidewalk	Park Ave	West	691	N	4.00	4.00	3.80	3.80
Herman St	Holly St	Railroad	East	1,296	Y	4.00	4.00	3.80	3.80
Walnut St	Audubon Avenue	Stoney Creek Park	South	4,808	N	4.00	4.00	3.80	3.80
Ash St	Stoney Creek	Spence St	North	1,011	Y	0.00	0.00	0.00	0.00
Grantham St	George St	William St	North	2,062	N	4.00	4.00	0.00	3.80
Holly St	Leslie Street	Lionel St	North	443	N	4.00	4.00	0.00	3.80
Slocumb St	Day Circle	Seymour Dr	North	445	Y	4.00	4.00	3.80	3.80
Wayne Memorial Dr	Ninth St	US 70	South/ East	1,125	Y	4.00	4.00	0.00	3.80
Wayne Memorial Dr	Hospital Rd	New Hope Rd	West	4,283	N	0.00	4.00	0.00	3.80
Daisy St	Park Ave	Sidewalk	East	131	N	4.00	4.00	3.80	3.80



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Fills an Existing Sidewalk Gap</i>	<i>Community College within 1/2 Mile</i>	<i>Footpath Present</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	TOTAL
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.94
	4.80	0.00	4.20	0.00	4.10	4.00	0.00	4.30	0.00	4.40	0.00	0.00	41.90
	4.80	0.00	4.20	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.74
	4.80	0.00	4.20	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	41.74
	4.80	0.00	0.00	4.30	4.10	4.00	0.00	4.30	0.00	0.00	4.44	3.88	41.62
	4.80	0.00	0.00	4.30	4.10	4.00	0.00	4.30	0.00	0.00	4.44	3.88	41.62
	4.80	0.00	0.00	4.30	4.10	4.00	0.00	4.30	0.00	0.00	4.44	0.00	41.54
	4.80	0.00	0.00	4.30	4.10	4.00	0.00	4.30	0.00	4.40	0.00	0.00	41.50
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	0.00	3.88	41.38
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	4.30	0.00	4.40	0.00	3.88	38.38
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	4.44	0.00	38.14
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	38.04
	4.80	0.00	0.00	4.30	0.00	0.00	0.00	4.30	0.00	4.40	4.44	0.00	37.84
	4.80	0.00	4.20	0.00	0.00	4.00	0.00	4.30	0.00	4.40	0.00	0.00	37.80
	4.80	4.40	4.20	0.00	4.10	4.00	0.00	4.30	4.10	0.00	0.00	0.00	37.70
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	37.54



SIDEWALK PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Side	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location	
Kornegay St	Magnolia St	Park Ave	East	510	N	4.00	4.00	3.80	3.80	
Daisy St	Sidewalk	Holly St	East	664	N	4.00	4.00	3.80	3.80	
Public Dr	Sidewalk	Slocumb St	North	791	N	4.00	4.00	3.80	3.80	
Holly St	Jefferson Ave	Sidewalk	North	794	N	4.00	4.00	3.80	3.80	
Harris St	Ben Brewington Ct	Stoney Creek Pkwy	South	3,995	N	4.00	4.00	3.80	3.80	
Olivia Ln	John St	Deveraux St	South	2,426	N	4.00	4.00	3.80	3.80	
Slocumb St	Wisteria Rd	Seymour Dr	North	518	N	4.00	4.00	3.80	3.80	
Ash St	Spence Ave	Berkeley Blvd	North	1,463	Y	0.00	0.00	0.00	0.00	
Berkeley Blvd	Elm St	Ash St	West	1,677	Y	0.00	0.00	0.00	0.00	
Berkeley Blvd	Ash St	Cashwell Dr	East/ South	2,211	Y	0.00	0.00	0.00	0.00	
Spence Ave	Ash St	US 70	West	7,174	Y	0.00	0.00	3.80	0.00	
Spence Ave	Ash St	US 70	East	7,183	Y	0.00	0.00	3.80	0.00	
George St	Holly St	Grantham St	East	410	N	4.00	4.00	0.00	3.80	
Poplar St	Weaver Pl	Sidewalk	East	80	N	4.00	4.00	0.00	3.80	
Bunche Dr	John St	Sycamore St	North	1,316	N	4.00	4.00	0.00	3.80	
Dixie Trail	Proposed Shared-Use Path	Slocumb St	North	1,494	N	4.00	4.00	0.00	3.80	
Bunche Dr	Stadium Rd	Slocumb St	North	986	N	4.00	4.00	0.00	3.80	
Harris St	Slocumb St	Poplar St	South	1,289	N	4.00	4.00	0.00	3.80	0
Weaver Dr	Slocumb St	Poplar St	North	1,294	N	4.00	4.00	0.00	3.80	
William St	McIntire St	Atlantic Ave	East	267	Y	4.00	4.00	0.00	3.80	
Pineview Ave	Evergreen Ave	Walnut St	West	488	N	4.00	4.00	3.80	3.80	
Pineview Ave	Evergreen Ave	Walnut St	East	494	N	4.00	4.00	3.80	3.80	



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Fills an Existing Sidewalk Gap</i>	<i>Community College within 1/2 Mile</i>	<i>Footpath Present</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	37.54
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	37.54
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	37.54
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	0.00	0.00	37.40
	4.80	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	4.44	3.88	37.22
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	4.44	0.00	37.14
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	4.30	0.00	0.00	4.44	3.88	36.92
	4.80	4.40	4.20	4.30	0.00	4.00	0.00	4.30	0.00	4.40	0.00	0.00	34.70
	4.80	4.40	4.20	4.30	0.00	4.00	0.00	4.30	0.00	4.40	0.00	0.00	34.70
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	4.30	0.00	4.40	0.00	0.00	34.50
	4.80	4.40	4.20	4.30	0.00	4.00	0.00	0.00	0.00	4.40	0.00	0.00	34.20
	4.80	4.40	4.20	4.30	0.00	4.00	0.00	0.00	0.00	4.40	0.00	0.00	34.20
	4.80	4.40	0.00	0.00	0.00	0.00	0.00	4.30	0.00	0.00	4.44	0.00	34.04
	4.80	4.40	0.00	0.00	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	33.84
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	4.44	0.00	33.84
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	4.44	0.00	33.84
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	33.74
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	4.44	0.00	33.74
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	4.40	0.00	0.00	33.70
	4.80	0.00	0.00	4.30	0.00	0.00	0.00	4.30	0.00	0.00	4.44	0.00	33.44
	4.80	0.00	0.00	4.30	0.00	0.00	0.00	4.30	0.00	0.00	4.44	0.00	33.44



SIDEWALK PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Side	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Walnut St	Oleander Ave	Andrews Ave	North	186	N	4.00	4.00	3.80	3.80
Slocumb St	Seymour Dr	Proposed Shared-Use Path	North	900	N	4.00	4.00	3.80	3.80
Berkeley Blvd	Elm St	Ash St	East	1,715	Y	0.00	0.00	0.00	0.00
Cashwell Dr	Berkeley Blvd	Malloy St	South/ West	2,184	Y	0.00	0.00	0.00	0.00
Ash St	Spence Ave	Berkeley Blvd	South	1,463	N	0.00	0.00	0.00	0.00
Main St	Russell Dr	Mill St	South	1,666	N	0.00	0.00	3.80	0.00
Main St	Russell Dr	Mill St	North	1,672	N	0.00	0.00	3.80	0.00
Berkeley Blvd	Cashwell Dr	Langston Dr	West/ North	1,518	N	0.00	0.00	0.00	0.00
Berkeley Blvd	Cashwell Dr	Fallin Blvd	East/ South	6,169	Y	0.00	0.00	0.00	0.00
Berkeley Blvd	Mall Access	New Hope Rd	West/ North	6,850	Y	0.00	0.00	0.00	0.00
George St	Brownrigg St	US 70	East	1,699	N	4.00	4.00	0.00	3.80
Wayne Memorial Dr	New Hope Rd	Cassedale Dr	East	306	N	0.00	4.00	0.00	3.80
Country Day Rd	Patetown Rd	Wayne Memorial Dr	West	6,535	N	0.00	4.00	0.00	3.80
Walnut St	Oleander Ave	Andrews Ave	North	88	N	4.00	4.00	3.80	3.80
US 70 Bypass Side Rd	William St	Eleventh St	North	5,575	N	4.00	4.00	0.00	3.80
Mill St	Main St	Vail St	West	312	N	0.00	0.00	0.00	0.00
Main St	Railroad St	Goldsboro St	North	403	N	0.00	0.00	0.00	0.00
Parkway Dr	Sidewalk	Berkeley Blvd	South	1,417	N	0.00	0.00	0.00	0.00
Ash St	Berkeley Blvd	Greenwood MS	North	3,056	N	0.00	0.00	0.00	0.00
Royall Ave	Spence St	Berkeley Blvd	North	4,577	Y	0.00	0.00	0.00	0.00
George Street	Railroad	US 70	West	3,158	Y	4.00	4.00	0.00	3.80



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Fills an Existing Sidewalk Gap</i>	<i>Community College within 1/2 Mile</i>	<i>Footpath Present</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.3	0.00	0.00	0.00	0.00	33.10
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	4.44	3.88	32.62
	4.80	4.40	4.20	4.30	0.00	4.00	0.00	4.30	0.00	4.40	0.00	0.00	30.40
	4.80	4.40	0.00	4.30	4.10	4.00	0.00	4.30	0.00	4.40	0.00	0.00	30.30
	4.80	4.40	4.20	4.30	0.00	4.00	0.00	4.30	0.00	0.00	0.00	0.00	30.30
	4.80	0.00	0.00	4.30	0.00	4.00	4.60	4.30	0.00	0.00	4.44	0.00	30.24
	4.80	0.00	0.00	4.30	0.00	4.00	4.60	4.30	0.00	0.00	4.44	0.00	30.24
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	4.30	0.00	0.00	0.00	0.00	30.10
	0.00	4.40	4.20	4.30	4.10	4.00	0.00	0.00	0.00	4.40	0.00	0.00	29.70
	0.00	4.40	4.20	4.30	4.10	4.00	0.00	0.00	0.00	4.40	0.00	0.00	29.70
	4.80	0.00	0.00	4.30	0.00	0.00	0.00	4.30	0.00	0.00	4.44	0.00	29.64
	4.80	4.40	4.20	0.00	4.10	0.00	0.00	0.00	4.10	0.00	0.00	0.00	29.40
	4.80	4.40	0.00	0.00	4.10	4.00	0.00	0.00	4.10	0.00	0.00	0.00	29.20
	4.80	0.00	0.00	0.00	4.10	0.00	0.00	4.30	0.00	0.00	0.00	0.00	28.80
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	0.00	0.00	0.00	28.60
	4.80	4.40	0.00	4.30	0.00	4.00	4.60	4.30	0.00	0.00	0.00	0.00	26.40
	4.80	4.40	0.00	4.30	0.00	4.00	4.60	4.30	0.00	0.00	0.00	0.00	26.40
	4.80	4.40	0.00	4.30	4.10	4.00	0.00	4.30	0.00	0.00	0.00	0.00	25.90
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	0.00	0.00	0.00	0.00	0.00	25.80
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	4.40	0.00	0.00	25.50
	4.80	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40	4.44	0.00	25.44



SIDEWALK PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Side	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
US 117	Big Daddy's Rd	High School	East	4,404	N	0.00	0.00	0.00	0.00
John St	Holly St	Atlantic Ave	West	312	N	4.00	4.00	0.00	3.80
Wayne Memorial Dr	Windsor Creek Pkwy	Tommy's Rd	East	1,527	N	0.00	4.00	0.00	3.80
Eleventh St	Lincoln Mercury Dr	Wayne Memorial Dr	West	163	N	4.00	4.00	0.00	3.80
Wayne Memorial Dr	Cassedale Dr	Best Ave	East	654	N	0.00	4.00	0.00	3.80
US 117	Mt Carmel Church Rd	Big Daddys Rd	East	3,371	N	0.00	0.00	0.00	0.00
Cuyler Best Rd	US 70	New Hope Rd	West/ North	6,475	N	0.00	0.00	0.00	0.00
Holly St	James St	Center St	South	411	N	4.00	4.00	0.00	3.80
Neil St	William St	Tuskegee St	South	645	N	0.00	4.00	0.00	3.80
Dixie Trail	John St	Proposed Shared-Use Path	North	2,198	N	4.00	4.00	0.00	3.80
Central Heights Rd	New Hope Rd	Sidewalk	East	270	N	0.00	0.00	0.00	0.00
Parkway Dr	North Park Dr	Sidewalk	South	880	N	0.00	0.00	0.00	0.00
New Hope Rd	Berkeley Blvd	Central Heights	South	3,761	N	0.00	0.00	3.80	0.00
Harding Dr	Proposed Shared-Use Path	New Hope Rd	North	4,699	N	0.00	0.00	3.80	0.00
Patetown Rd	Country Day Rd	Kearney Ln	West	2,804	N	0.00	4.00	0.00	3.80
Mill St	Vail St	Church St	West	359	N	0.00	0.00	0.00	0.00
Mill St	Church St	School St	West	548	N	0.00	0.00	0.00	0.00
Malloy St	Ash St	Cashwell Dr	East	1,461	N	0.00	0.00	0.00	0.00
Central Heights Rd	New Hope Rd	US 13	West	6,254	N	0.00	0.00	0.00	0.00
Central Heights Rd	New Hope Rd	US 13	East	6,265	N	0.00	0.00	0.00	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Fills an Existing Sidewalk Gap</i>	<i>Community College within 1/2 Mile</i>	<i>Footpath Present</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	4.30	0.00	0.00	0.00	0.00	25.40
	4.80	0.00	0.00	0.00	4.10	0.00	0.00	4.30	0.00	0.00	0.00	0.00	25.00
	0.00	4.40	4.20	0.00	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	24.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	24.60
	0.00	4.40	4.20	0.00	4.10	0.00	0.00	0.00	4.10	0.00	0.00	0.00	24.60
	4.80	4.40	0.00	4.30	0.00	4.00	0.00	0.00	0.00	0.00	0.00	0.00	21.80
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.10	0.00	0.00	0.00	21.70
	4.80	0.00	0.00	0.00	0.00	0.00	0.00	4.30	0.00	0.00	0.00	0.00	20.90
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	20.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	20.30
	4.80	4.40	0.00	0.00	4.10	0.00	0.00	4.30	0.00	0.00	0.00	0.00	17.60
	4.80	0.00	0.00	4.30	4.10	0.00	0.00	4.30	0.00	0.00	0.00	0.00	17.50
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	16.60
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	16.60
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	16.30
	4.80	0.00	0.00	4.30	0.00	0.00	0.00	4.30	0.00	0.00	0.00	0.00	13.40
	4.80	0.00	0.00	4.30	0.00	0.00	0.00	4.30	0.00	0.00	0.00	0.00	13.40
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	12.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	12.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	12.80



SIDEWALK PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Side	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
New Hope Rd	Central Heights Rd	Food Lion	North	862	N	0.00	0.00	0.00	0.00
Walnut Creek Dr	Dogwood Tr	Mill Rd	South	494	N	0.00	0.00	0.00	0.00
Walnut Creek Dr	Mill Rd	Tramway Dr	South	664	N	0.00	0.00	0.00	0.00
Walnut Creek Dr	Breezewood Dr	Dogwood Trail	South	688	N	0.00	0.00	0.00	0.00
Doral Dr	Pinehurst Ln	Breezewood Dr	East	1,009	N	0.00	0.00	0.00	0.00
Doral Dr	Pinehurst Ln	Walnut Creek Dr	West	1,175	N	0.00	0.00	0.00	0.00
Walnut Creek Dr	Tramway Dr	US 70	East	2,908	N	0.00	0.00	0.00	0.00
Walnut Creek Dr	Breezewood Dr	US 70	West	5,043	N	0.00	0.00	0.00	0.00
Walnut Creek Dr	End of Road	Doral Dr	North/ West	5,273	N	0.00	0.00	0.00	0.00
Lakeshore Dr	Lakeshore Dr	Mill Rd	South/ West	5,353	N	0.00	0.00	0.00	0.00
Lakeshore Dr	Lakeshore Dr	Mill Rd	North/ East	5,439	N	0.00	0.00	0.00	0.00
Walnut Creek Dr	End of Road	Doral Dr	South/ East	5,452	N	0.00	0.00	0.00	0.00
Mill Rd	Lakeshore Dr	Walnut Creek Dr	North/ West	10,860	N	0.00	0.00	0.00	0.00
Mill Rd	Lakeshore Dr	Walnut Creek Dr	South/ East	11,122	N	0.00	0.00	0.00	0.00
Pinehurst Ln	End of Road	Doral Dr	North	1,278	N	0.00	0.00	0.00	0.00
Pinehurst Ln	End of Road	Doral Dr	South	1,301	N	0.00	0.00	0.00	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Fills an Existing Sidewalk Gap</i>	<i>Community College within 1/2 Mile</i>	<i>Footpath Present</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	0.00	4.40	0.00	0.00	4.10	4.00	0.00	0.00	0.00	0.00	0.00	0.00	12.50
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40



ON-ROAD BICYCLE PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Facility Type	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Mulberry St	William St	Randolph St	Bicycle Boulevard	8,973	4.00	4.00	3.80	3.80	4.30
Virginia St	Pine St	Murray St	Bicycle Boulevard	5,656	4.00	4.00	3.80	3.80	4.30
Center St	Spruce St	Mulberry St	Bike Lane	1,510	4.00	4.00	3.80	3.80	4.30
Center St	Spruce St	Mulberry St	Bike Lane	1,511	4.00	4.00	3.80	3.80	4.30
Wayne Memorial Dr	Holly St	New Hope Rd	Wide Outside Lanes	14,136	4.00	4.00	3.80	3.80	4.30
Ash Street/NC 581	Old Smithfield Rd	Virginia St	Paved Shoulders	8,953	4.00	4.00	3.80	3.80	4.30
Mulberry St	James St	William St	Shared Lane Markings	1,635	4.00	4.00	0.00	3.80	4.30
Center St	Ash St	Oak St	Bike Lane	479	4.00	4.00	3.80	3.80	0.00
Center St	Ash St	Oak St	Bike Lane	481	4.00	4.00	3.80	3.80	0.00
Slocumb St	Elm St	Ash St	Shared Lane Markings	3,234	4.00	4.00	0.00	3.80	4.30
Slocumb St	Ash St	Simmons St	Bicycle Boulevard	315	4.00	4.00	0.00	3.80	4.30
Slocumb St	Seymour Johnson AFB	Elm St	Bike Lane	10,962	4.00	4.00	3.80	3.80	4.30
Madison Ave	Laurel St	Atlantic Ave	Bicycle Boulevard	6,237	4.00	4.00	3.80	3.80	4.30
Elm St	Center St	Randolph St	Shared Lane Markings	9,643	4.00	4.00	3.80	3.80	4.30
Audubon Ave	Olivia Ln	Atlantic Ave	Bicycle Boulevard	7,172	4.00	4.00	3.80	3.80	0.00
Herman St	Elm St	Mulberry St	Bicycle Boulevard	2,654	4.00	4.00	3.80	3.80	4.30
Harris St	Slocumb St	Porter St	Shared Lane Markings	3,355	4.00	4.00	3.80	3.80	4.30
Stronach Ave	Greenleaf St	Humphrey St	Bicycle Boulevard	3,418	4.00	4.00	3.80	3.80	4.30
Holly St	Railroad	Herman St	Bicycle Boulevard	3,946	4.00	4.00	3.80	3.80	4.30
Edgerton St	Maple St	Claiborne St	Bicycle Boulevard	4,892	4.00	4.00	3.80	3.80	4.30



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	4.80	4.40	4.20	4.30	4.10	4.00	4.60	0.00	4.44	0.00	54.74
	0.00	4.40	4.20	4.30	4.10	4.00	4.60	0.00	4.44	0.00	49.94
	4.80	4.40	4.20	0.00	4.10	4.00	4.60	0.00	0.00	3.88	49.88
	4.80	4.40	4.20	0.00	4.10	4.00	4.60	0.00	0.00	3.88	49.88
	0.00	4.40	4.20	4.30	4.10	4.00	0.00	4.10	4.44	0.00	49.44
	0.00	4.40	4.20	4.30	0.00	4.00	4.60	0.00	4.44	0.00	45.84
	4.80	4.40	4.20	0.00	4.10	4.00	4.60	0.00	0.00	0.00	42.20
	4.80	4.40	4.20	0.00	0.00	4.00	4.60	0.00	0.00	3.88	41.48
	4.80	4.40	4.20	0.00	0.00	4.00	4.60	0.00	0.00	3.88	41.48
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	4.44	3.88	41.22
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	4.44	3.88	41.22
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	4.44	0.00	41.14
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	3.88	41.02
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	3.88	41.02
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	4.44	3.88	40.72
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	37.14
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	37.14
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	37.14
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	37.14
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	37.14



ON-ROAD BICYCLE PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Facility Type	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Olivia Ln	John St	Audubon Ave	Bicycle Boulevard	5,700	4.00	4.00	3.80	3.80	4.30
John St	Arrington Bridge Rd	Elm St	Paved Shoulders	11,593	4.00	4.00	3.80	3.80	4.30
Ninth St	Humphrey St	Jefferson Ave	Bicycle Boulevard	2,945	0.00	4.00	3.80	3.80	4.30
Lockhaven Dr	Wayne Memorial Dr	Gloucester Rd	Bicycle Boulevard	982	4.00	4.00	3.80	3.80	4.30
Jackson St	Mulberry St	Maple St	Bicycle Boulevard	3,595	4.00	4.00	3.80	3.80	0.00
Berkeley Blvd	Elm St	Tommy's Rd	Wide Outside Lanes	19,506	0.00	0.00	0.00	0.00	4.30
Harris St	John St	Slocumb St	Bike Lane	3,182	4.00	4.00	0.00	3.80	4.30
Clingman St	Holly St	Stronach Ave	Bike Lane	2,704	0.00	4.00	3.80	3.80	4.30
Best St	Stephens St	Laurel St	Bicycle Boulevard	4,791	4.00	4.00	3.80	3.80	4.30
NC Highway 111	Goldsboro MPO Limits	New Hope Rd	Paved Shoulders	48,471	4.00	0.00	0.00	3.80	4.30
William St	Patetown Rd	Stronach Ave	Bike Lane	4,934	4.00	4.00	0.00	3.80	4.30
Jefferson Ave	Ninth St	Banks Ave	Bicycle Boulevard	575	4.00	4.00	3.80	3.80	0.00
Peachtree St	Claiborne St	Durant St	Bicycle Boulevard	1,130	4.00	4.00	3.80	3.80	0.00
Humphrey St	Fourth St	Ninth St	Bicycle Boulevard	1,802	4.00	4.00	3.80	3.80	0.00
Claiborne St	Edgerton St	Peachtree St	Bicycle Boulevard	2,357	4.00	4.00	3.80	3.80	0.00
Beech St	Center St	Claiborne St	Bicycle Boulevard	9,483	4.00	4.00	3.80	3.80	0.00
George St	Murray St	A St	Shared Lane Markings	1,538	4.00	4.00	0.00	3.80	4.30
Old Mount Olive Rd	Goldsboro MPO Limits	Old Grantham Rd	Paved Shoulders	37,965	4.00	4.00	0.00	3.80	4.30
Nelson Dr	North Dr	Cardinal Dr	Bicycle Boulevard	288	4.00	4.00	0.00	3.80	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/ Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	37.14
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	37.14
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	4.44	0.00	37.14
	0.00	4.40	0.00	0.00	4.10	4.00	0.00	0.00	4.44	0.00	36.84
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	3.88	36.28
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	0.00	4.44	0.00	34.54
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	33.34
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	33.14
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	33.04
	0.00	4.40	0.00	0.00	4.10	4.00	0.00	0.00	4.44	3.88	32.92
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	0.00	32.90
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	32.84
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	32.84
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	32.84
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	32.84
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	32.84
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	29.24
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	29.24
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	29.04



ON-ROAD BICYCLE PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Facility Type	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
North Dr	Banks Ave	Nelson Dr	Bicycle Boulevard	319	4.00	4.00	0.00	3.80	0.00
North Dr	Saxon St	Nelson Dr	Bicycle Boulevard	962	4.00	4.00	0.00	3.80	0.00
Simmons St	Slocumb St	Lionel St	Bicycle Boulevard	985	4.00	4.00	0.00	3.80	0.00
Banks Ave	Jefferson Ave	North Dr	Bicycle Boulevard	1,344	4.00	4.00	0.00	3.80	0.00
Cardinal Drive	Nelson Dr	Quail Dr	Bicycle Boulevard	2,375	4.00	4.00	0.00	3.80	0.00
Mulberry St	Alabama Ave	James St	Bicycle Boulevard	2,398	4.00	4.00	0.00	3.80	0.00
Lionel St	Simmons St	Holly St	Bicycle Boulevard	1,662	4.00	4.00	0.00	3.80	4.30
Quail Dr	North Dr	Cardinal Dr	Bicycle Boulevard	3,400	0.00	4.00	3.80	3.80	0.00
Westbrook Rd	Arrington Bridge Rd	Slocumb St	Paved Shoulders	6,166	4.00	4.00	0.00	3.80	0.00
Fussell St	George St	James St	Bicycle Boulevard	655	4.00	4.00	3.80	3.80	0.00
Swan St	James St	Greenleaf St	Bicycle Boulevard	1,202	4.00	4.00	3.80	3.80	0.00
Center St	Holly St	Swan St	Bicycle Boulevard	2,159	4.00	4.00	3.80	3.80	0.00
Newsome St	Slaughter St	Poplar St	Bicycle Boulevard	309	4.00	4.00	3.80	3.80	0.00
Slaughter St	Newsome St	Elm St	Bicycle Boulevard	1,984	4.00	4.00	3.80	3.80	0.00
Elm St	George St	Center St	Buffered Bike Lane	997	4.00	4.00	0.00	3.80	0.00
Jefferson Ave	Edgerton St	Ninth St	Bicycle Boulevard	1,248	4.00	4.00	3.80	3.80	0.00
Center St	Elm St	Spruce St	Bike Lane	1,014	4.00	4.00	0.00	3.80	0.00
Center St	Elm St	Spruce St	Bike Lane	1,014	4.00	4.00	0.00	3.80	0.00
Harris St	Porter St	Stoney Creek Pkwy	Bike Lane	2,643	4.00	4.00	3.80	3.80	0.00
Old Grantham Rd	Goldsboro MPO Limits	Old Mount Olive Hwy	Paved Shoulders	32,128	4.00	4.00	0.00	0.00	4.30



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	TOTAL
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	29.04
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	29.04
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	29.04
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	29.04
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	29.04
	0.00	4.40	4.20	0.00	4.10	0.00	0.00	0.00	4.44	0.00	28.94
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	28.90
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	28.84
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	3.88	28.82
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	28.74
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	28.74
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	28.74
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	4.44	0.00	28.54
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	4.44	0.00	28.54
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	3.88	28.48
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	28.40
	0.00	4.40	4.20	0.00	4.10	0.00	0.00	0.00	0.00	3.88	28.38
	0.00	4.40	4.20	0.00	4.10	0.00	0.00	0.00	0.00	3.88	28.38
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	3.88	28.32
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	25.44



ON-ROAD BICYCLE PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Facility Type	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Sleepy Creek Rd	US Highway 117 Alternate	Goldsboro MPO Limits	Paved Shoulders	31,673	0.00	4.00	0.00	3.80	4.30
Spence St	Cashwell Dr	US 70	Wide Outside Lanes	5,642	0.00	0.00	3.80	0.00	4.30
Greenleaf St	Swan St	Freeman St	Bicycle Boulevard	339	4.00	4.00	0.00	3.80	0.00
Oberry Rd	Goldsboro MPO Limits	US Highway 117 Alternate	Paved Shoulders	14,443	4.00	4.00	0.00	3.80	4.30
North Dr	The First Church Rd	Quail Dr	Bicycle Boulevard	615	4.00	4.00	0.00	3.80	0.00
Gloucester Rd	Lockhaven Dr	Glen Oak Dr	Bicycle Boulevard	1,153	4.00	4.00	0.00	3.80	0.00
Poplar St	Newsome St	Weaver Dr	Bicycle Boulevard	1,950	4.00	4.00	0.00	3.80	0.00
Elm St	Randolph St	Berkeley Blvd	Bike Lane	3,954	4.00	0.00	0.00	3.80	4.30
Murray St	Virginia St	George St	Bicycle Boulevard	505	4.00	4.00	3.80	3.80	0.00
Holly St	Oak St	George St	Bicycle Boulevard	2,340	4.00	4.00	3.80	3.80	0.00
Stephens St	Ben Brewington Ct	End of Road	Bicycle Boulevard	3,577	4.00	4.00	3.80	3.80	0.00
Mimosa St	Pineview Ave	Randolph St	Bicycle Boulevard	3,976	4.00	4.00	3.80	3.80	0.00
Arrington Bridge Rd	John St	Westbrook Rd	Paved Shoulders	3,467	4.00	4.00	0.00	3.80	4.30
Cuyler Best Rd	North Park Dr	New Hope Rd	Paved Shoulders	5,270	0.00	0.00	0.00	0.00	0.00
Main St	Fort St	Mill St	Bike Lane	657	0.00	0.00	3.80	0.00	0.00
Oak Forest Dr	Central Heights Rd	Seymour Johnson AFB	Paved Shoulders	12,155	4.00	0.00	0.00	0.00	4.30
Wayne Memorial Dr	New Hope Rd	Lanetown Rd	Paved Shoulders	43,107	0.00	0.00	0.00	0.00	4.30
Ash St	Malloy St	Oak Forest Rd	Bike Lane	3,370	4.00	0.00	0.00	0.00	0.00
Dollard Town Rd	NC Highway 111	Lake Wackena Rd	Paved Shoulders	10,047	4.00	0.00	0.00	3.80	4.30



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	TOTAL
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	4.44	0.00	25.04
	0.00	4.40	4.20	4.30	0.00	4.00	0.00	0.00	0.00	0.00	25.00
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	24.94
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	24.94
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	4.44	0.00	24.74
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	4.44	0.00	24.74
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	4.44	0.00	24.74
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	3.88	24.48
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	24.44
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	24.44
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	24.44
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	24.44
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	3.88	24.38
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	4.10	0.00	0.00	21.70
	0.00	4.40	0.00	4.30	0.00	4.00	4.60	0.00	0.00	0.00	21.10
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	21.10
	0.00	4.40	4.20	0.00	4.10	0.00	0.00	4.10	0.00	0.00	21.10
	0.00	4.40	4.20	4.30	4.10	0.00	0.00	0.00	0.00	0.00	21.00
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	20.94



ON-ROAD BICYCLE PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Facility Type	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Cashwell Dr	Berkeley Blvd	Berkeley Memorial Park	Bike Lane	2,655	4.00	0.00	0.00	0.00	0.00
Tommy's Rd	NC Highway 111	Central Heights Rd	Paved Shoulders	21,543	0.00	4.00	0.00	3.80	0.00
Ben Brewington Ct	Stephens St	End of Road	Bicycle Boulevard	677	4.00	4.00	0.00	3.80	0.00
Cashwell Dr	Hillcrest Dr	Berkeley Blvd	Bike Lane	2,285	0.00	0.00	3.80	0.00	0.00
Patetown Rd	William St	New Hope Rd	Paved Shoulders	6,185	0.00	4.00	0.00	3.80	0.00
Pecan Rd	Old Mount Olive Highway	Arrington Bridge Rd	Paved Shoulders	12,426	0.00	4.00	3.80	3.80	0.00
Country Day Rd	Patetown Rd	Wayne Memorial Dr	Paved Shoulders	6,552	0.00	4.00	0.00	3.80	0.00
Arrington Bridge Rd	Westbrook Rd	Bill Lane Blvd	Paved Shoulders	15,004	0.00	4.00	0.00	3.80	4.30
Center St	Oak St	Holly St	Bicycle Boulevard	1,581	4.00	4.00	3.80	3.80	0.00
Main St	Northeast Railroad St	Goldsboro St	Bike Lane	479	0.00	0.00	0.00	0.00	0.00
Main St	Mill St	Northeast Railroad St	Shared Lane Markings	609	0.00	0.00	0.00	0.00	0.00
Big Daddy's Rd	Goldsboro St	Town Limits	Bike Lane	1,047	0.00	0.00	0.00	0.00	0.00
Main St	Interstate 795	Fort St	Shared Lane Markings	2,179	0.00	0.00	3.80	0.00	0.00
New Hope Rd	Berkeley Blvd	Millers Chapel Rd	Paved Shoulders	15,386	0.00	0.00	0.00	0.00	4.30
Bryan Blvd	Old Grantham Rd	Neuse River	Paved Shoulders	2,736	4.00	4.00	0.00	0.00	0.00
NC Highway 581	Rosewood Rd	Buck Swamp Rd	Paved Shoulders	18,784	0.00	0.00	3.80	0.00	4.30
Salem Church Rd	George St	Pikeville-Princeton Rd	Paved Shoulders	36,956	0.00	4.00	0.00	3.80	4.30
NC Highway 111	Mount Carmel Church Rd	New Hope Rd	Paved Shoulders	12,754	0.00	4.00	0.00	3.80	0.00
Holly St	George St	Railroad	Bike Lane	1,143	4.00	4.00	0.00	3.80	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	TOTAL
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	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	0.00	20.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	4.44	0.00	20.74
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	20.64
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	0.00	20.60
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	20.60
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	20.44
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	4.10	0.00	0.00	20.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	3.88	20.38
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	20.00
	0.00	4.40	0.00	4.30	0.00	4.00	4.60	0.00	0.00	0.00	17.30
	0.00	4.40	0.00	4.30	0.00	4.00	4.60	0.00	0.00	0.00	17.30
	0.00	4.40	0.00	4.30	0.00	4.00	4.60	0.00	0.00	0.00	17.30
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	0.00	16.94
	0.00	4.40	0.00	0.00	4.10	4.00	0.00	0.00	0.00	0.00	16.80
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	16.70
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	16.60
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	16.50
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	16.30
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	16.20



ON-ROAD BICYCLE PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Facility Type	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Bill Lane Blvd	Arrington Bridge Rd	NC Highway 111	Paved Shoulders	10,944	4.00	0.00	0.00	3.80	0.00
Hare Rd	New Hope Rd	Wayne Memorial Dr	Paved Shoulders	9,316	0.00	0.00	0.00	0.00	0.00
Malloy St	Ash St	Cashwell Dr	Bike Lane	1,519	0.00	0.00	0.00	0.00	0.00
Dollard Town Rd	Lake Wackena Rd	St. John Church Rd	Paved Shoulders	9,592	0.00	0.00	0.00	0.00	4.30
Central Heights Rd	Oak Forest Dr	Tommy's Rd	Paved Shoulders	12,785	0.00	0.00	0.00	0.00	4.30
Rosewood Rd	Old Smithfield Rd	Ash St	Paved Shoulders	36,796	0.00	0.00	0.00	0.00	4.30
Ridgewood Dr	Ash St	Peachtree St	Bicycle Boulevard	447	0.00	0.00	0.00	0.00	0.00
Big Daddy's Rd	Pikeville Town Limits	Airport Rd	Paved Shoulders	8,282	0.00	0.00	0.00	0.00	0.00
Ferry Bridge Rd	Old Smithfield Rd	Goldsboro MPO Limits	Paved Shoulders	14,481	0.00	0.00	0.00	0.00	4.30
Piney Grove Church Rd	Beston Rd	Seven Springs Town Limits	Paved Shoulders	25,818	0.00	0.00	0.00	0.00	0.00
Peachtree St	Ridgewood Dr	Hillcrest Dr	Bicycle Boulevard	1,294	0.00	0.00	3.80	0.00	0.00
Hillcrest Dr	Peachtree St	Cashwell Dr	Bicycle Boulevard	1,334	0.00	0.00	3.80	0.00	0.00
Spring Bank Rd	Bill Lane Blvd	NC Highway 111	Paved Shoulders	6,319	4.00	0.00	0.00	3.80	0.00
Lakeshore Dr	Lake Wackena Rd	Mill Rd	Bicycle Boulevard	1,721	0.00	0.00	0.00	0.00	0.00
Walnut Creek Dr	Mill Rd	US 70	Bicycle Boulevard	3,680	0.00	0.00	0.00	0.00	0.00
Mill Rd	Lake Shore Dr	Walnut Creek Dr	Bicycle Boulevard	10,988	0.00	0.00	0.00	0.00	0.00
Gurley Dairy Rd	Capps Bridge Rd	NC Highway 581	Paved Shoulders	11,940	0.00	0.00	0.00	0.00	4.30
Buck Swamp Rd	NC Highway 581	Salem Church Rd	Paved Shoulders	16,901	0.00	0.00	0.00	0.00	4.30
St. John Church Rd	Dollard Town Rd	Piney Grove Church Rd	Paved Shoulders	8,823	0.00	0.00	0.00	0.00	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	TOTAL
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	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	3.88	16.08
	4.80	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	13.30
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	12.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	12.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	12.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	12.80
	0.00	4.40	0.00	4.30	0.00	4.00	0.00	0.00	0.00	0.00	12.70
	0.00	4.40	0.00	4.30	0.00	4.00	0.00	0.00	0.00	0.00	12.70
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	3.88	12.58
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	3.88	12.58
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	12.50
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	12.50
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	12.20
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.70
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	8.50



ON-ROAD BICYCLE PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Facility Type	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
New Hope Road	Millers Chapel Road	Beston Road	Paved Shoulders	13,425	0.00	0.00	0.00	0.00	0.00
Parkstown Road	New Hope Road	Goldsboro MPO Limits	Paved Shoulders	24,275	0.00	0.00	0.00	0.00	0.00
NC 581	Gurley Dairy Road	Buck Swamp Road	Paved Shoulders	2,765	4.00	0.00	0.00	0.00	0.00
Arrington Bridge Road	Bill Lane Boulevard	Sleepy Creek Road	Paved Shoulders	18,594	0.00	0.00	0.00	3.80	0.00
Old Smithfield Road	Ferry Bridge Road	Rosewood Road	Paved Shoulders	1,556	0.00	0.00	0.00	0.00	0.00
Pikeville-Princeton Road	Nahunta Road	Interstate 795	Paved Shoulders	1,987	0.00	0.00	0.00	0.00	0.00
Lake Wackena Road	Dollard Town Road	Lake Shore Drive	Paved Shoulders	4,740	0.00	0.00	0.00	0.00	0.00
Airport Road	Big Daddy's Road	Mount Carmel Church Road	Paved Shoulders	6,937	0.00	0.00	0.00	0.00	0.00
Stoney Creek Church Road	NC Highway 111	Wayne Memorial Drive	Paved Shoulders	11,635	0.00	0.00	0.00	0.00	0.00
Mount Carmel Church Road	Airport Road	NC Highway 111	Paved Shoulders	14,589	0.00	0.00	0.00	0.00	0.00
Beston Road	New Hope Road	Piney Grove Church Road	Paved Shoulders	20,541	0.00	0.00	0.00	0.00	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	TOTAL
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	8.50
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	8.50
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.20
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40



SHARED-USE PATH PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Georgia Avenue Trail	Elm St	Holly St	6,092	4.00	4.00	3.80	3.80	0.00
Willow Dale Cemetery-Ash "Ditch" Trail	Willow Dale Cemetery	Ash St	5,445	4.00	4.00	3.80	3.80	4.30
Ash-Peacock Park "Ditch" Trail	Ash St	Peacock Park	5,871	4.00	4.00	3.80	3.80	0.00
Wayne Country Day-Hospital Trail	Wayne Country Day School	Royall Avenue (via Wayne Memorial Hospital & Cuyler Best/Spence)	11,708	4.00	4.00	3.80	3.80	0.00
Slocumb Street Sidepath	Stoney Creek Greenway South	Westbrook Rd	2,844	4.00	4.00	3.80	3.80	4.30
Stoney Creek Greenway Connector	Stoney Creek Greenway North	Stoney Creek Park (across Ash)	744	4.00	4.00	0.00	3.80	4.30
Ash Sidepath	Stoney Creek	Ridgewood Dr	338	4.00	4.00	0.00	3.80	0.00
Westbrook Road Sidepath	Slocumb St	Arrington Bridge Rd	5,073	4.00	4.00	0.00	3.80	4.30
Fairview Park Connector	Fairview Park	Stoney Creek Greenway North	2,950	4.00	4.00	3.80	3.80	0.00
South End Neighborhood Spur	Stoney Creek Greenway South	South End Public Housing	1,169	4.00	4.00	3.80	3.80	0.00
Poplar-Slocumb Connector	Poplar St	Slocumb St	3,644	4.00	4.00	0.00	3.80	0.00
Elm-Olivia Trail	Elm St	Olivia Ln	1,532	4.00	4.00	3.80	3.80	4.30
Walnut-Stoney Creek Connector	Walnut St	Stoney Creek Greenway	222	4.00	4.00	0.00	3.80	0.00
Brick-Elm Connector	Brick St	Elm St	2,121	4.00	4.00	0.00	3.80	0.00
Willow Dale Cemetery-Westbrook Trail	Willow Dale Cemetery	Westbrook Rd	10,827	4.00	4.00	0.00	3.80	0.00
Rocky Branch Greenway Connector	Rocky Branch Greenway	Wayne Country Day-Hospital Trail	306	4.00	4.00	0.00	3.80	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	4.80	4.40	4.20	4.30	4.10	0.00	4.60	0.00	4.44	3.88	50.32
	4.80	4.40	0.00	4.30	4.10	4.00	0.00	0.00	4.44	3.88	49.82
	4.80	4.40	0.00	4.30	4.10	4.00	0.00	0.00	4.44	3.88	45.52
	0.00	4.40	4.20	4.30	4.10	4.00	0.00	4.10	4.44	0.00	45.14
	4.80	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	3.88	41.72
	4.80	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	3.88	41.58
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	0.00	0.00	3.88	41.48
	4.80	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	3.88	37.92
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	37.64
	4.80	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	3.88	37.42
	4.80	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	3.88	33.62
	4.80	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	33.54
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	3.88	33.28
	4.80	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	3.88	33.28
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	3.88	32.92
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	4.10	0.00	0.00	32.70



SHARED-USE PATH PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Stoney Creek-Berkeley Boulevard Trail	Stoney Creek Greenway North	Berkeley Blvd	6,959	0.00	0.00	0.00	0.00	4.30
Beech-Mountain Bike Park Connector	Beech St	Mountain Bike Park	277	4.00	4.00	0.00	3.80	0.00
George Street-Willow Dale Cemetery Trail	George St	Willow Dale Cemetery	3,288	4.00	4.00	0.00	3.80	0.00
Stoney Creek Greenway South	Elm St	Slocumb St	9,007	4.00	4.00	0.00	3.80	0.00
Stoney Creek Greenway North Extension	Royall Ave	US Highway 70	2,477	4.00	4.00	0.00	3.80	0.00
Quail Park Spur	Quail Dr	Stoney Creek Greenway North Extension	715	4.00	4.00	0.00	3.80	0.00
New Hope Road Sidepath	Patetown Rd	Cassedale Dr	8,852	0.00	4.00	0.00	3.80	0.00
Mountains to Sea Trail-George Street Connector	Mountains to Sea Trail (west of Brick Street)	George St (via Waynesborough Park)	5,063	4.00	4.00	0.00	3.80	0.00
Quaker Neck Lake-Brick Road Trail	Quaker Neck Lake	Brick Rd	12,560	4.00	4.00	0.00	3.80	0.00
Gloucester-Hospital Spur	Gloucester Rd	Wayne Country Day-Hospital Trail	457	4.00	4.00	0.00	3.80	0.00
Cuyler Best-YMCA Connector	Cuyler Best Rd	YMCA	3,709	0.00	0.00	0.00	0.00	0.00
New Hope Road Sidepath	Existing sidepath	Berkeley Blvd	2,681	0.00	0.00	3.80	0.00	4.30
East End Spur	Elm-Olivia Trail	East End Cir	179	4.00	4.00	0.00	3.80	0.00
The First Church Road-Wayne Memorial Drive Connector	The First Church Rd	Wayne Memorial Dr	1,387	4.00	4.00	0.00	3.80	0.00
Westbrook-Neuse River MST Trail Segment	Westbrook Rd	Neuse River	3,483	4.00	4.00	0.00	3.80	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	<i>TOTAL</i>
	4.80	4.40	4.20	4.30	4.10	4.00	0.00	0.00	0.00	0.00	30.10
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	29.04
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	4.44	0.00	29.04
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	4.44	3.88	28.82
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	0.00	28.60
	0.00	4.40	0.00	4.30	4.10	0.00	0.00	0.00	0.00	0.00	24.60
	0.00	4.40	0.00	0.00	4.10	4.00	0.00	4.10	0.00	0.00	24.40
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	3.88	24.38
	0.00	4.40	0.00	4.30	0.00	0.00	0.00	0.00	0.00	3.88	24.38
	0.00	4.40	0.00	0.00	4.10	4.00	0.00	0.00	0.00	0.00	24.30
	4.80	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	0.00	21.60
	4.80	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	21.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	4.44	0.00	20.64
	0.00	4.40	0.00	0.00	0.00	4.00	0.00	0.00	0.00	0.00	20.20
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	3.88	20.08



SHARED-USE PATH PRIORITIZATION

Yellow highlighting = Previously identified as City priority

Roadway	From	To	Length (Feet)	Low-Income	Low Vehicle Areas	High Density Population	Minority Population	Pedestrian/Bicycle Crash Location
Little River Trail Segment 2	Windy Ridge Drive	Mountains to Sea Trail (via Cherry Experimental Farm property)	44,228	4.00	4.00	0.00	3.80	0.00
New Hope Road Sidepath	Existing sidepath	New Hare Drive	669	0.00	0.00	0.00	0.00	0.00
Berkeley-Greenwood-Meadow Lane Trail	Berkeley Boulevard	Meadow Lane Elementary/Future Soccer Park (via Berkeley Memorial Park & Greenwood Middle School)	9,905	0.00	0.00	0.00	0.00	0.00
Wayne Christian Academy-Wayne Country Day Connector	Wayne Christian Academy/Patetown Road	Wayne Country Day School	5,139	0.00	4.00	0.00	3.80	0.00
Little River Trail Segment 1	County Line	Windy Ridge Drive	59,822	0.00	0.00	0.00	0.00	0.00
Little River Trail - Salem Church Road Spur	Little River Trail Segment 2	Salem Church	7,181	0.00	0.00	0.00	0.00	0.00



	<i>Connects to Existing Facility</i>	<i>Connects to Proposed Facility</i>	<i>Top 5 Need from Public Comments</i>	<i>Park, library, or recreation center within 1/2 mile</i>	<i>Grade school within 1/2 mile</i>	<i>Connects to Shopping/Business Areas</i>	<i>Connects to Downtown</i>	<i>Community College within 1/2 Mile</i>	<i>Health Focus Area</i>	<i>Connects to MST Alignment</i>	TOTAL
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	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	3.88	20.08
	4.80	4.40	0.00	0.00	4.10	0.00	0.00	4.10	0.00	0.00	17.40
	0.00	4.40	0.00	4.30	4.10	4.00	0.00	0.00	0.00	0.00	16.80
	0.00	4.40	0.00	0.00	4.10	0.00	0.00	0.00	0.00	0.00	16.30
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40
	0.00	4.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	4.40

